

Andover Conservation Area Design Statement

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Portus +
Whitton

landscape architects

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Section I: Baseline Conditions

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In parallel with ‘the Vision for Andover’ report which focuses at a Masterplan level the redevelopment of the Chantry Centre and the more peripheral areas of the town, this Design Statement has been developed at a more detailed level for the central core of the Conservation Area. Its primary purpose is to bring a consistent and appropriate quality to the surfaces and street furniture that respects and enhances the historic architectural setting. In one particular location, where the Conservation Area meets the proposed redevelopment of the Chantry Centre, the report also makes proposals as to how these can be successfully integrated.

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I. Introduction

I.1 Introduction

Andover has been the subject of a broad reaching review which has looked at a number of significant changes to the wider road network and its infrastructure of buildings particularly the Chantry Centre and the north-west quarter.

To help realise this potential for enhancement, Andover Town Council have instigated the development of a Design Statement for the Conservation Area, to ensure that all future public realm schemes are sympathetic to Andover's heritage as well as appropriate and cohesive with one another. In this way, the aspiration is that this document will act as a design guide to achieve, over time, a consistency and quality to the surfaces and street furniture both within the Conservation Area itself but also the help inform the language of materials and design within the proposed re-development of the north-west quarter of the town.

I.2 Purpose of the Design Statement

To that end, Portus + Whitton have been commissioned by Andover Town Council to produce a concise a Design Statement for the public realm within Andover's Conservation Area. This is to ensure an overarching Design Statement exists to be referred to when considering all future public realm developments.

The Design Statement is in three sections,; its baseline condition, its current condition in terms of its detailing and the proposals. This is in the form of a series of recommendations and design templates that can be developed in detail and then applied over time as and when further redevelopment takes place. Because all the elements that contribute to the streetscape are closely related to the buildings, this Design Statement includes where relevant, proposals for building frontages where an integrated approach would be of most benefit to link the historic core to the refurbishment proposals.

Once in place, this will then enable Andover Town Council to set design standards to ensure new development is both consistent and appropriate to the character of the conservation area and is an attractive, desirable town centre with a unique identity.

2. Scope of Study Area

2.1 Introduction

The scope of the recommendations are limited to the streets within the historic core of the boundaries of the conservation area. Within this defined area, previous public consultation has confirmed that the significant majority of visitors to the town centre arrive by private car and therefore their route, From the point where they leave the car park to gain access to the town centre, is via the lanes and passageways.

In terms of the visitor experience, this therefore begins at the point where the lane or passageway starts and this becomes the perceived point of arrival. As a result, this Design Statement focuses on these routes and how they can provide an enhanced sense of arrival and legibility to visitors.

2.2 Scope of Work

The design and specification of paving and the associated kerbs and channels are heavily Influenced by vehicular traffic: this includes the frequency and nature of the vehicles from commercial deliveries, refuse lorries, private vehicles, buses, taxis, mobility scooters and bicycles. In certain locations, this Design Statement also make suggestions relating to changes to the existing vehicle flow and parking arrangements which would contribute to the wider master planning process for Andover.

2.3 Scope of Study Area

Key

The Chantry Centre - Existing layout - Only the area within the Conservation Area is included within the scope of proposals

Area designated for development. Not included within the scope of proposals

Opportunities for Green / Public Space

Scope of proposals

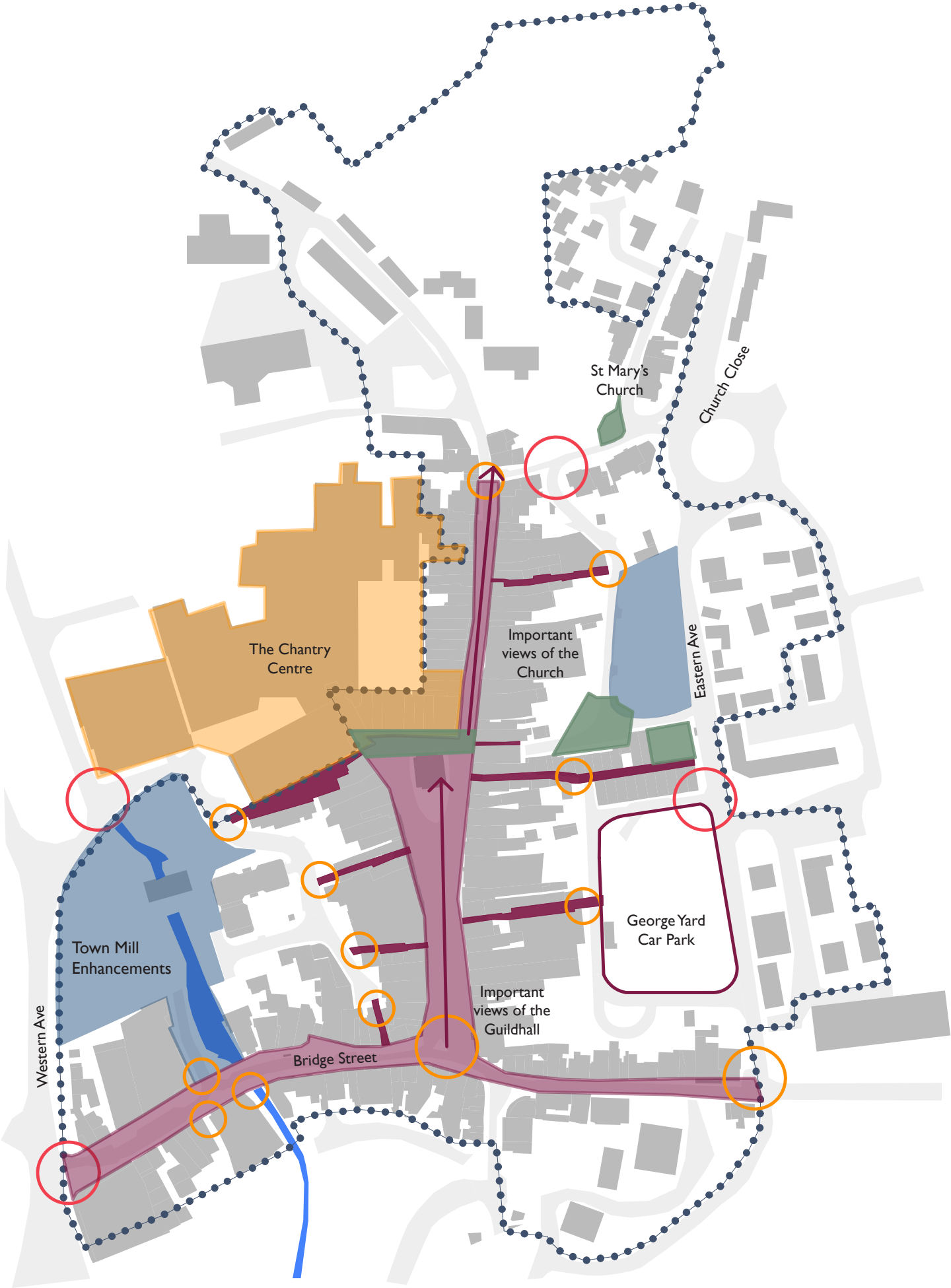
Existing Car Parks to be retained

Pedestrian gateways into the town centre

Vehicular gateways into the town centre

Important Views

Conservation Area - Site Boundary



3. Setting and History

3.1 Town Location and Setting

Andover is a market town located in the county of Hampshire, to the South of England.

The town sits along the River Anton, a major source of the Test River. The river played an important role in the development of the town shaping its mill and milling history, which were first noted in the Domesday Book of 1086. Relics of the industry are present to this day, including The Town Mills, now a pub, situated prominently within the conservation area.

Andover sits relatively close to and more or less equidistantly from other towns and cities, Winchester, also in Hampshire and Salisbury and Marlborough in Wiltshire. In the context of these historic and popular visitor destinations, the future appearance of Andover's public realm can be an important component to express its unique identity and encourage visits.

The town has a longstanding history of being an important stop over destination, attributed to its location at the intersection of cities such as London and Exeter, as well as Oxford and Southampton.

3.2 Historical Development

This section provides a glimpsed overview of the history of the town and how developments have shaped Andover over time, resulting in the built form as seen today.

Andover was first recorded in 950 as Andeferas, thought to be of Celtic origin, meaning “ash tree water”, when it is thought King Edred built a royal hunting lodge in the town.

By the time of the 1086 Domesday Book, Andover's population had expanded to an estimated 500 inhabitants with a well established milling industry, grinding grain to flour using water power from the river Anton.

In 1175, King Richard I negotiated a charter with the townspeople to form a merchant guild, which elected bailiff officials to run the town. These authorities grew when in 1201 King John gave the merchants a right to collect royal taxes and further again in 1256, Henry III gave the townspeople the right to hold a court and try criminals for offenses committed within Andover. This was further developed in 1599 when Elizabeth I honored the town with a new charter, enabling the merchants guild to form a corporation

The town was partially destroyed by two fires, one in 1141 and the next in 1435.

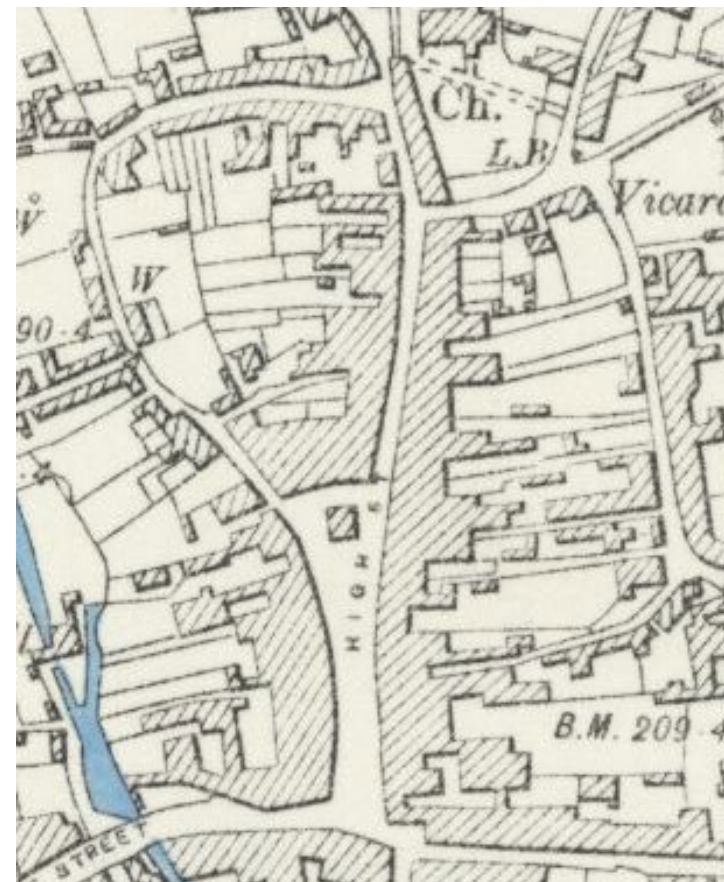
Over the next 400 years or so, Andover's popularity grew as a result of its central location and as a stopover on the Exeter to London road. A canal to Southampton was built in the 1780s highlighting the river's prominence in the development of the town, but was closed in 1859, replaced with a railway. The legacy of its location can be seen today in the number of coaching inns present within the conservation area including The Star and Garter, The Globe and The White Hart.

In 1809, Taskers Waterloo Ironworks opened, the heart of which was a forge. Taking advantage of the opportunities the Industrial Revolution and expansion of the railways provided, the ironworks expanded, widening its product range by 1865 to include steam engines. Taskers designed and built many engines, but demand ebbed and flowed as a consequence of events such as the First World War, with the last steam engine leaving the factory in 1927. From that point until its closure in the 1990s, Taskers (then Taskers of Andover) became known for their lorry trailers. The legacy of the company can still be found in Andover with street names such as Taskers Drive and Foundry Road.

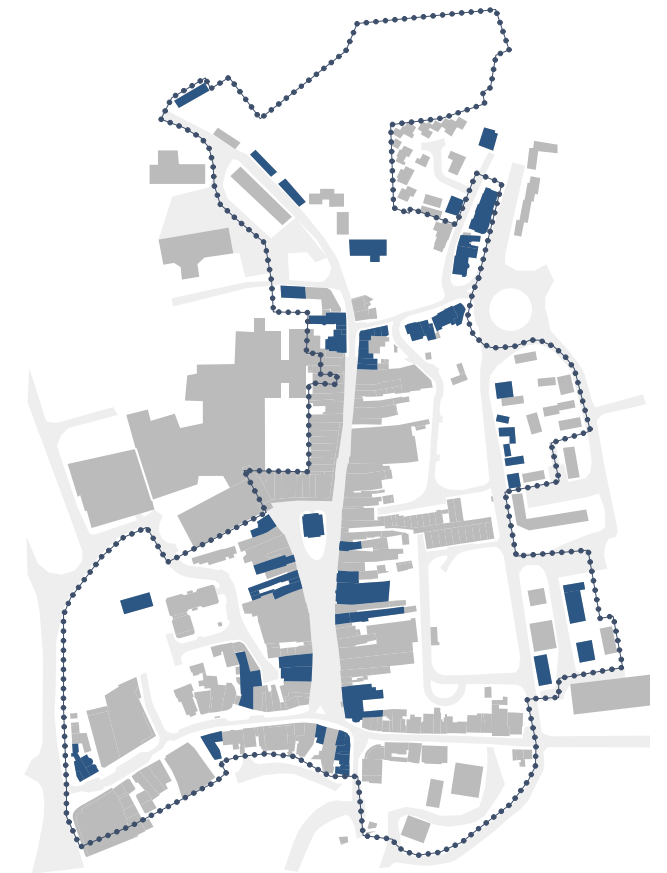
In 1969, the original conservation area was designated, an area with “...special architectural or historic interest, the character and appearance of which it is desirable to preserve or enhance....” The boundaries were extended in 1983, The area encompasses significant streets and roads



Andover Historic OS 1888 Andover



Andover Historic 1842-1952 High Street



The listed buildings within Andover Conservation Area

including Bridge Street and London Street running in a west/east direction and the High Street and a section of the A3057 along a north/south axis. The conservation area contains over 40 Grade II listed buildings.

Andover also benefits from its central location and close proximity to surrounding historical attraction. These include Stonehenge, Weyhill (home of Weyhill Fair, one of England's oldest medieval fairs), Silchester (originally Calleva an Iron Age settlement that was conquered by Romans in AD43) and Portway, a Roman road between modern day Silchester and Old Sarum.

3.3 Assets

In terms of its buildings and public open space, Andover has many strong features which need to be considered as part of the Design Statement. These act as a benchmark for quality for all future public realm developments and ensure that future proposals are complementary to and an enhancement of the historic core. These assets include but are not limited to:

3.3.1 The Guildhall:

The impressive Georgian and Grade II* listed building is situated at the heart of the conservation area and is the keystone building within the town centre. Assisted by the gentle incline in gradient as you ascend the High Street, the Guildhall is the central viewpoint from Bridge Street along the High Street. However, the close proximity of The Guildhall's current backdrop, The Chantry Centre, detracts from its splendid exterior.

3.3.2 Riverside Location:

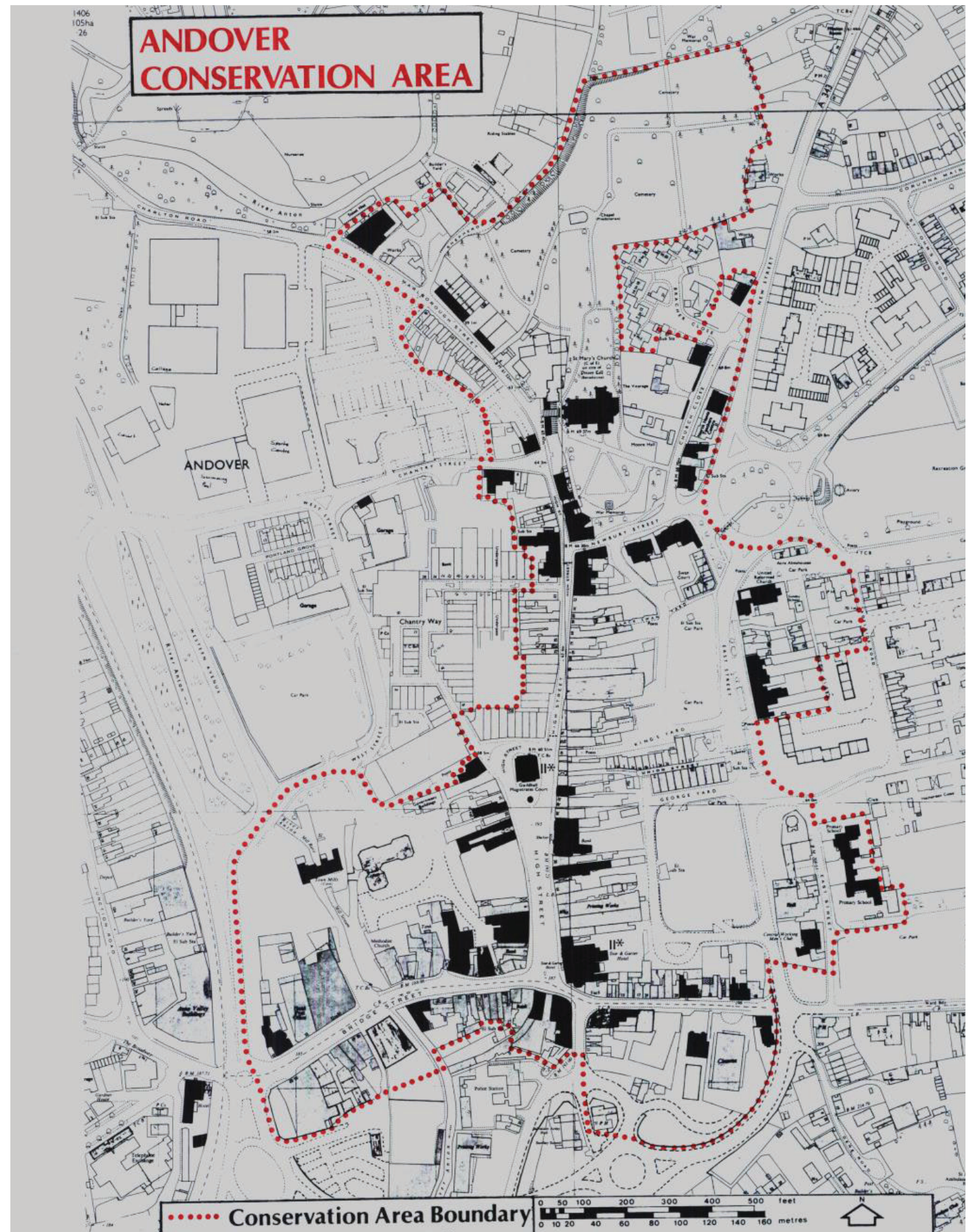
The river is a vital artery and destination within the conservation area, with many utilising the river side location for recreation and pleasure.

3.3.3 Public Art:

Elements of sculpture and other forms of art including mosaic populate the town and provide elements of visual interest to enhance the unique character and history of the town.

3.3.4 Green Infrastructure:

The Town Mills riverside space, alongside pockets of green space within the conservation area provide a strong foundation in which to help build an identity for Andover as a town known for its green infrastructure. Enhanced by good quality planting along the periphery as well as new developments which offer the potential for additional further planting, these can be a contributory step towards eliminating the carbon footprint which is an aim of Test Valley Borough Council, who declared a Climate Emergency in September 2019.



4. Settlement Patterns

4.1 Settlement Patterns

The Conservation Area is made up of a number of historic streets and modern roads. The High Street being the most important central spine running North to South and Bridge Street leading to London Street running West to East. Please note: For the purposes of clarity within this report, the art of the High Street that runs from the Guildhall to St Mary's Church is referred to as the Upper High Street.

4.2 Open and Green Spaces

Andover Cemetery and The Town Mills open space are the two main areas of green open space within the conservation area. They are important spaces to this central part of Andover. In addition to these, there are a number of large trees within the Conservation Area, principally the main ones being the Plane tree in front of the Chantry Centre and the large mature trees, copper Beech, Beech and Sycamore etc. next to the Church on Bridge Street.

Proposals to enhance the tree planting within the Conservation area have been provided within this Design Statement.

Design Guidelines

- The historic skyline with the Guildhall being the tallest of the buildings should be maintained.
- The existing green spaces are important and should be maintained
- 'Succession' planting to replace aging trees should be implemented before existing trees start to become hazardous.



5. Key Views and Vistas

5.1 Key Views and Vistas

The Andover Conservation Area encompasses an historic core, with the Guildhall at the centre. Looking at the historic buildings, the Guildhall stands out on its own and is the tallest of all the buildings.

Adjacent building heights should not exceed the existing ridge heights in order to maintain the prominence of the Guildhall.

The prominent tower of St Marys church benefits from its elevated position and rightly forms a landmark from various locations, most notably from the upper high street.

Views from the Guild Hall are also of merit, the natural stone apron of the building forming a natural viewpoint towards the picturesque facades and shop frontages of Bridge Street.

In addition to these principal views, the layout of streets and landmark buildings together form numerous picturesque centrally framed or glimpsed views.

Where buildings of a more prosaic nature are present, these can be visually softened by street tree planting particularly where mutual benefits for framing views can be achieved.



Key view looking North towards the Guildhall up the High Street.



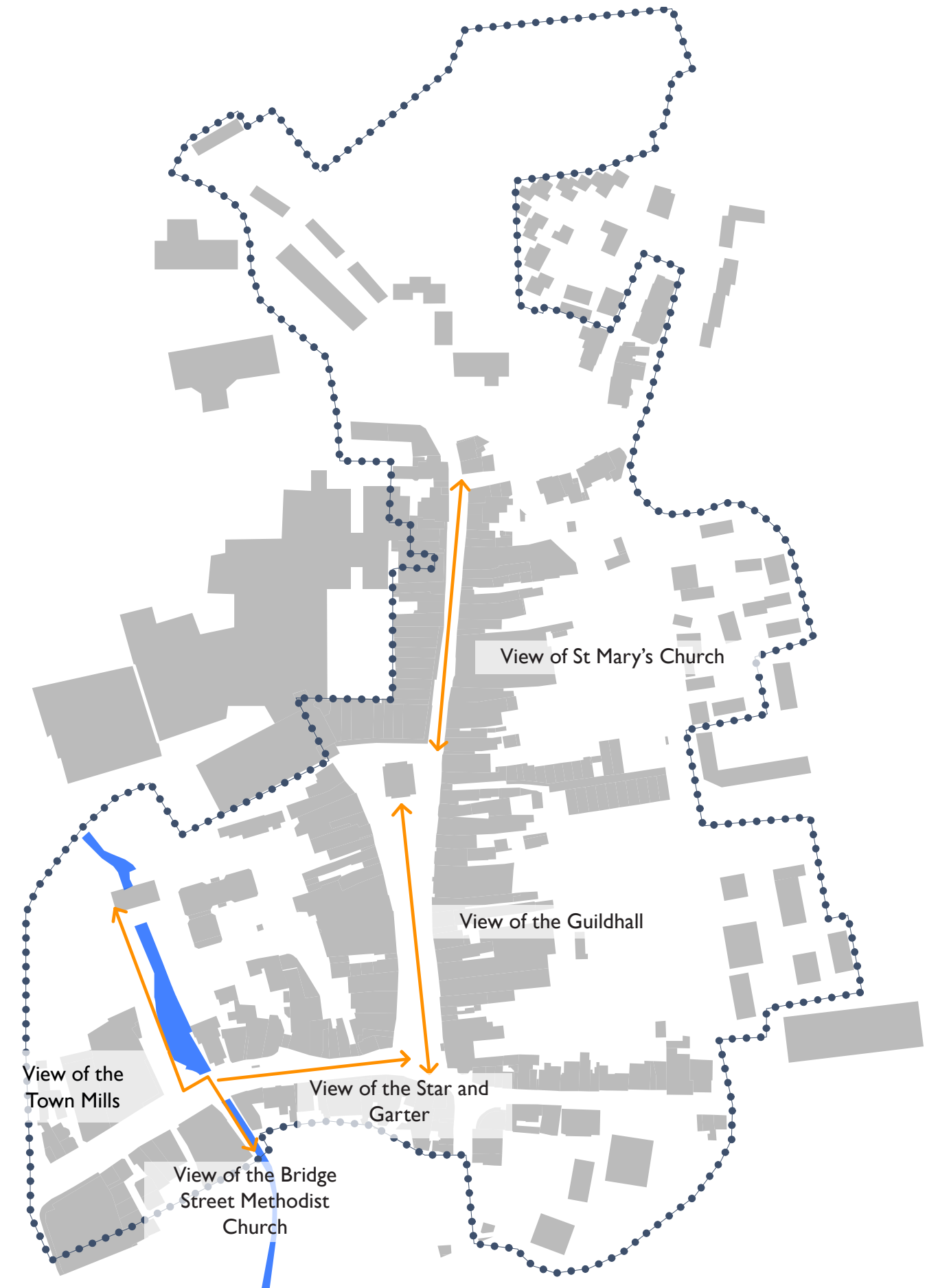
Key view looking North towards St Mary's Church up the High Street.



Key view looking along the River Anton towards the Town Mills and the Methodist Church



Key view looking eastwards along Bridge Street



Design Guidelines

- Maintain views of the guildhall from the southern end of the High Street.
- Maintain Views of St Mary's Church from key points along of the High Street.
- Maintain views of the Town Mills along the riverside.
- Maintain views of the Methodist Church from Bridge Street . Remove the Loading bays in front of the church.

Section 2: Detailed Condition

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While the historic core of Andover benefits from both a generous number of individually listed buildings, it is their collective value when seen together that forms the distinctive character of the town. In contrast to this, there are frequent instances where the quality of the streets and street furniture impoverish the overall quality of the town. Separate initiatives to improve the streetscape have, over time, created a sometimes uncomfortable juxtaposition of styles

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6. Built Form

6.1 Introduction

While the views within the High Street are largely dominated by the Guildhall, the wider appeal of the High Street is generated by the range and generally high quality of the buildings that frame it. These form a hierarchy of styles and materials from the stone facades and pedimented Palladian architecture (Barclays Bank), to the painted render with stone and high quality timber joinery of the windows including bow windows and doorways (Star and Garter) though to brick. These are typically enhanced with ashlar stone dressings to the reveals, parapet roof lines or deep, painted eaves. Where the traditional scale of glazing and doorways are retained and illuminated signage is restrained, shop frontages at ground floor form an enhancement to the overall character. Stone porticoes with decorative ironwork add positively to the overall street scene.

While there is a significant diversity in the materials, scale and detailed treatment, when viewed collectively, it is this range and juxtaposition of the differing architectural styles that contribute greatly to the charm of the High Street.

6.1.1 Form and Mass

Within the High Street, buildings do not typically exceed 3 storeys although this varies towards the edges of the conservation area (see settlement map above). For example, towards the north end of the Upper High and Street, buildings are more commonly one to two storeys. Buildings of this height are also found along Bridge Street. Exceptions to this include the SimplyHealth building, which is an isolated six storey structure. However, due to its location closer to Town Mills, it does not set any form of precedence for buildings within the High Street

Other buildings are notable, not necessarily for their height but their footprint including The Chantry Centre, Sainsburys and Wilko, which are anomalies compared to the narrower structures within the historic core. Many buildings along the High Street, the Upper High Street and Bridge Street are typically allocated to retail on the lower floors with upper

levels as residential.

6.1.2 Fenestration

As with building form, mass and style, fenestration in the conservation area is varies widely. There are a number of the larger modern retail outlets where windows are not present at all, resulting in areas of tall, overbearing structures. A good example can be found in Waterloo Court Lane. The lack of windows and fenestration can often lead to the “back of house” feel experienced along George Lane and Waterloo Court.

However, good examples of windows with traditional lintels, sills and casings can be found across the High Street. Windows of these buildings are also typically sash windows. The historic buildings are also typically well fenestrated, one key area which separates successful buildings with less successful buildings (see The Star and Garter compared to Wilko/Poundland for a comparison). Window frames are typically painted in pale shades.

6.1.3 Features

Other features which work well within the conservation area include the arched windows of the Guildhall - a motif reflected on doorways along the High Street (such as at The Globe and Barclays bank), porticoes (at The Star and Garter), round bay windows (at The Globe and The Star and Garter), roof parapets (The White Hart) and also narrow second storey balconies with intricate ironwork railings.

6.1.4 Materiality

Exposed and painted brick buildings are commonly found throughout the conservation area, with red brick being the predominant material. This can also be found across historical buildings such as The Town Mills pub. Where red brick is used successfully, the colour is broken up with a pale coloured mortar.

Other areas where such brickwork is used, such as at Wiko, Iceland and at Poundstretcher are overbearing, as the single

Successful buildings:



The Town Mills benefits from light red brick with a light mortar; numerous windows as well as iron railings.



The Bridge Street Methodist Church employs flint for the exterior, making it a keystone building, complementary to the style of the wider area.



The sandstone Guildhall benefits from tall sash windows on the upper floor, with an arched variety on the ground floor.



The white painted brick exterior of The Globe is in keeping with other buildings along the High Street. Round bay windows, as well as sash windows and an arched doorway.



#32's use of ochre brick, with the arched doorway, sash windows with sills and lintels, in addition to a second storey balcony with railings.



The painted stone Star and Garter benefits from a wide portico, round bay and sash windows, as well as a second storey balcony with iron railings.

block of material is not balanced with the presence of windows, doors and signage.

Red brick buildings are interspersed between painted brick buildings, typically in soft pale shades including white, light grey and cream. In other areas, such as on the Bridge Street Methodist Church, traditional flint has been applied whereas ochre brick and sandstone, for example at The Redbridge and The Guildhall/Barclays respectively, have also been used

6.1.5 Roofline

The roofs within the historic core of the conservation area are of a relatively consistent pitch, often set back at this level and occasionally with dormers. Where buildings are less successful and detract from the conservation area, roofs are flat or are not in keeping with the traditional pitched construction.

Unsuccessful buildings:



Iceland (right) is composed of dark red brick with dark mortar joints. The roof pitch is not in keeping with others in the area. In addition, the windows and door frames are dark and in a modern style, and have poor rhythm. The use of bright red and yellow signage also detracts from the peaceful riverside location. The juxtaposition is clearly evident compared to The Bridge cafe (left).



Areas such as Waterloo Court Lane suffer from a lack of fenestration, leading to an overbearing feeling within the Lane.



The buildings pictured detract from the conservation area for a number of reasons, including their flat roof shape, "striped" brick effect and modern window shape and lack of sash effect. In addition the red bricks lack tonal variation.



Wilko's roof and window design are its greatest downfall, although the lighter coloured brick is less overbearing than a darker choice, important given the footprint of the building.

6.2 Proposed Built Form

Within the historic core of the town, new buildings should be in keeping with and respectful of their surrounding streetscape. New buildings should continue in a sympathetic manner the existing material and colour palette already found in the conservation area.

Careful detailing and observance of scale and proportion of windows and entrance doorways can acknowledge and be empathetic to their historic context without descending into pastiche.

6.2.1 Massing

Within the conservation area, no future building should exceed three and a half storeys which will ensure future developments do not overbear on the more historic core. The exception is directly to the north of the Guildhall, where as the principal building within the town, any new buildings behind it should be a maximum of two and a half storeys tall, in order that the form of the Guildhall is neither obfuscated nor left to compete against larger more overbearing facades.

In addition, new buildings should be sympathetic to the scale and mass of historic buildings found within the core of the conservation area, reflecting widths and heights appropriate to and no larger than similar historic styles found along the street.

6.2.2 Materials

Within the conservation area, any future buildings should employ similar materials and be applied in a similar manner as historic buildings within the conservation area. Consequently, the materials should typically:
Be red brick, sandstone or, on exception, flint.
Employ a similar traditional brick bonding pattern.
Use a pale lime mortar joint of appropriate thickness.

Where appropriate, buildings that are to be painted, should be of a pale colour such as white, cream or grey.

6.2.3 Fenestration

New buildings should be well fenestrated, with window size, number and rhythm well balanced and proportionate to the size of walls and facades.

Windows should be traditional in style, ideally sash windows with stone or painted wood lintels, casings and sills, if painted, these should typically be of a pale colour. The use of dark paint on lintels, sills and casings should be carefully considered.

Buildings should embrace the traditional pattern of retail on the lower floors, with upper floors reserved for residential purposes. Projecting colonnaded frontages offer a protected micro-climate for shoppers.

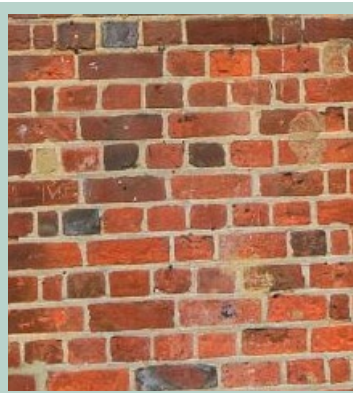
6.2.4 Features

- Building facades should be designed to include a variety of features already present on the buildings within the conservation area, to ensure visual appeal and prevent a plain, blocked appearance. These include:
- Arched window and door frames
- Porticoes
- Round bay windows at ground and upper floors
- Roof parapets
- Second storey balconies
- Delicate and detailed ironwork railings

6.2.5 Roofline

Roofs should be of a simple pitched design, with gables abutting the gables of the neighboring properties. If of an appropriate style, building facades might also make use of parapets along their front elevation. The use of chimneys would be appropriate to the conservation area and be formed of a simple stack and chimney pot structure.

Proposed Materials



Rustic red brick with tonal variation and pale mortar joints.



Sandstone

Fenestration



Sash windows with sandstone lintels and sills and white casings and sash bars



Round bay windows

Features



Ironwork balconies



Arched doorways



Porticoes

7. Surfacing

7.1 Existing Condition

There is a range of materials which differ in colour, textile and module size including high quality natural stone. In certain locations, for instance in the upper High street, there are 4 to 5 different materials across its width. While the pattern of materials is reminiscent of a road layout, the bench seating and, signage sometimes sits incongruously within the central ‘carriageway’ section.

In other locations, closer to Bridge street, the painted markings are of low quality and are degrading.

Examples of “in fill” materials which contrast the existing surfaces



Examples of areas which use multiple material choices and finishes



Examples of the varied material finishes across the conservation area



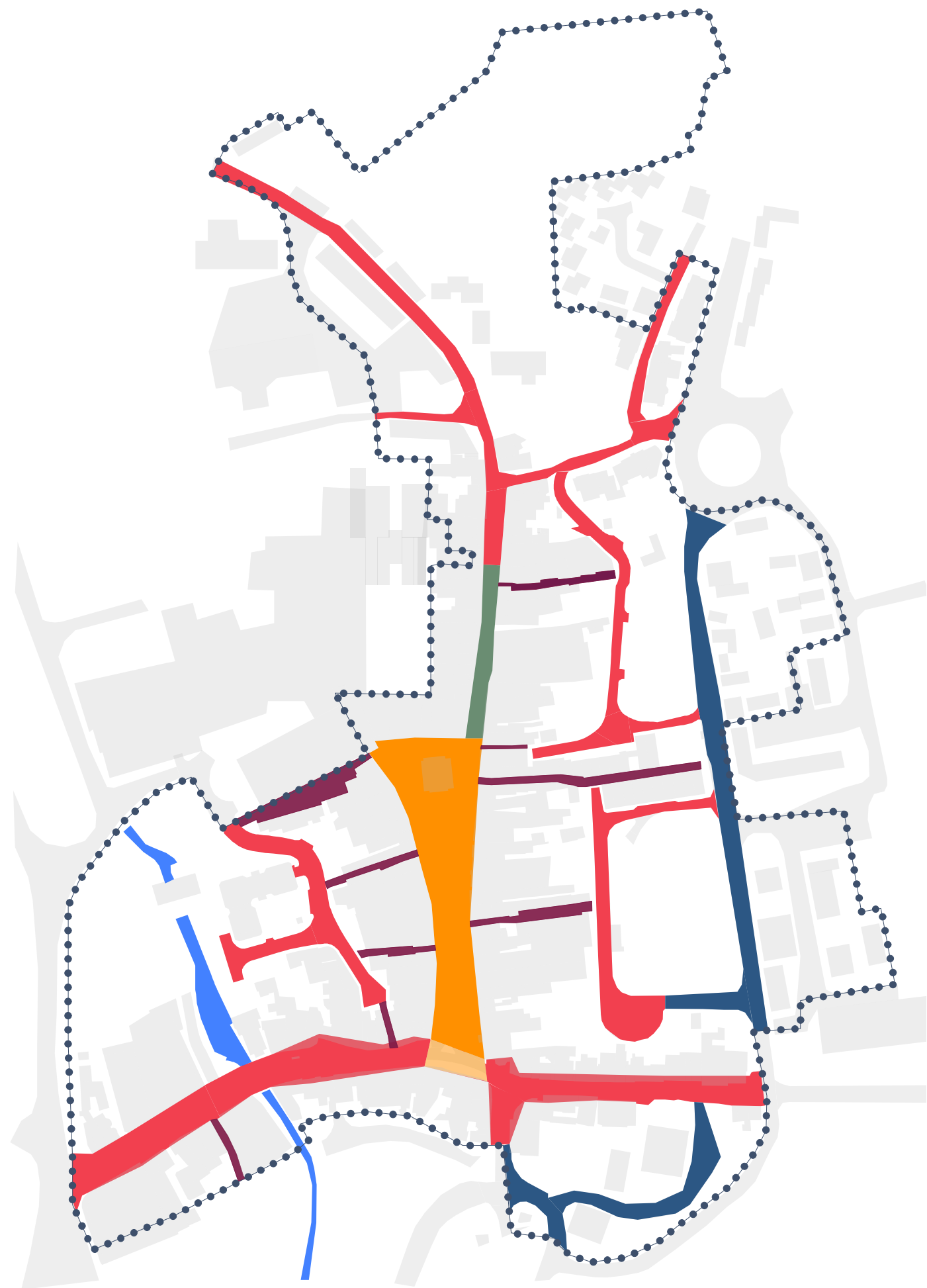
7.2 Proposed

The location plan of the town shows how the range of street types could be applied. These follow a hierarchy of quality which in broad terms is of the highest quality in the most prominent locations and reducing as they extend further away from the centre.

In addition, the following principles should be applied:
The use of paint for demarcation (for example for parking bays) should be avoided. Rather, such elements should be worked into the surfacing, for a higher quality finish.

Design Guidelines

- Any areas of patched surfacing should be removed and replaced with the correct surrounding surfacing.
- All manhole covers must be reassessed with correct and appropriately installed surfacing with it.



Proposed Surfacing Types

Street Type A

Natural stone flag paving on footway, natural stone setts, natural stone block paving carriageway.
e.g. High Street

Street Type B

Natural stone flag paving on footway, corduroy paving, wide-top conservation kerb, natural stone sett paved carriageway & channel
e.g. Bottom of the High Street

Street Type C

Natural stone flag paving on footway, wide-top conservation kerb, block channel & carriageway
e.g. Bridge Street

Street Type D

Natural stone flag paving on footway, natural stone sett paved carriageway & channel
e.g. Upper High Street

Street Type E - Lanes

e.g. Union Street and upper high street
Natural yorkstone setts

Street Type F

Block paving on footway, wide-top conservation kerb, tarmac & rolled chippings finish to carriageway
e.g. Eastern Avenue

8. Street Furniture

8.1 Introduction

Whilst not necessarily lacking in material quality individually, it is the range and close juxtaposition of different styles from polished or brushed, modern stainless-steel elements contrasted against more traditional features, that creates an underlying disharmony. This applies to most aspects of the existing street furniture from seating and signage to bins and bollards. Photographs of this are included within the section in their respective categories.

The following design principles should be followed when considering the implementation of any new street furniture within Andover’s conservation area.

8.2 Design Principles

Good quality street furniture is required to enhance the character of the town centre, not necessarily of a single uniform design but which belongs to the same family of design using common materials, colour, proportion but with a gradation of applied decoration that follows in broad terms, the hierarchy of the streets. Street furniture needs to be appropriate in number and location so as to not clutter the street scape and in order to provide amenity to residents without constraining access for servicing or flexibility to accommodate future events.

The Victorian Jubilee fountain and lamp is located in a central location in front of the Guildhall and features prominently in photographs of the town. For this reason, coupled with the town’s historic association with foundries and machine manufacture, this feature has been adopted as the precedent for street furniture within the high street.

The design of the street furniture should be coordinated through the consistent use of a painted dark or black metallic finish in traditional Victorian style. Gold detailing could be included where appropriate i.e. adjacent to the Victorian Jubilee fountain and lamp.

Furniture should be durable and easy to maintain. Its

traditional styling needs to be compliant with current legislation e.g. in the case of bollards, the inclusion of reflective strips to minimise hazards. This process would be part of a rationalisation exercise to take place before any new street furniture is installed, with any underused or visually obtrusive features removed.

8.2 Seats and Benches

8.2.1 Existing Condition

Benches can be found in great number across the conservation area. They are widely varied in style.

- These include but are not limited to:
- A timber “s” shaped backless bench alongside the river on Bridge Street.
- Stainless steel benches with curved edges along the High Street.
- Feature seating around the Plane tree in front of the Chantry Centre.
- Timber benches with black iron armrests along the riverside, in front of Wilkos.
- Backless timber benches, including at the pocket park end of the Waterloo Court lane.

While the conservation area currently benefits from a large number of benches, the majority of their styles and finishes is not in line with those you would expect to find within a historic town centre. Styles that are deemed especially at odds with this include backless benches, modern stainless-steel varieties and those of a serpentine nature when viewed in plan.

8.2.2 Proposed

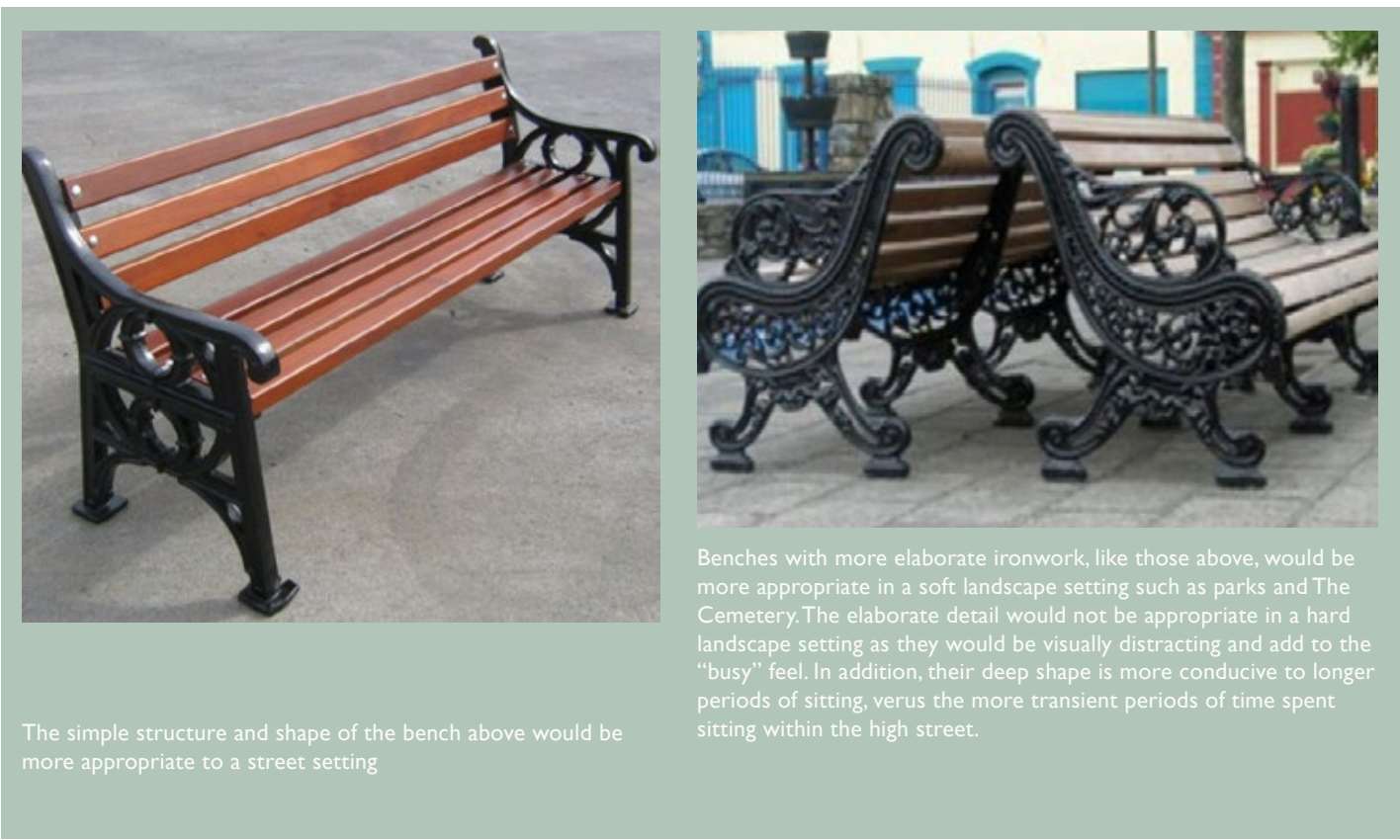
Metallic structures with traditional end styling and timber slatting provide robust and durable seating that are warm to sit on and quick to dry after rain.

To accord with the precedent Victorian Jubilee fountain, Andover Town Council have highlighted their preference for benches with elaborate cast iron armrests, as illustrated. In order to assist with space legibility and hierarchy as well as visual interest, a restricted range of styles are proposed – from ones that would be suited to an urban setting (such as streets and roads) with other complementary styles for green spaces, such as the pocket park abutting Waterloo Court and the Cemetery at St Mary’s Church.

Existing Unacceptable Benches



Proposed Benches



The simple structure and shape of the bench above would be more appropriate to a street setting

Benches with more elaborate ironwork, like those above, would be more appropriate in a soft landscape setting such as parks and The Cemetery. The elaborate detail would not be appropriate in a hard landscape setting as they would be visually distracting and add to the “busy” feel. In addition, their deep shape is more conducive to longer periods of sitting, versus the more transient periods of time spent sitting within the high street.

8.3 Bins

8.3.1 Existing Condition

A minimum of four different litter bin styles were identified including a simple, industrial style steel bin (the most commonly found), a black plastic square unit as well as a black plastic circular unit.

While the steel bins are inappropriate for a conservation area because of their industrial feel, the overtly curved edges and bold coloured openings of the recycling bins also appear incongruous against the historic backdrop. Although the design of the black units are inoffensive, their plastic moulded form detracts from their character.

The majority of the bins are not segregated and appeared to be for general waste only.

8.3.2 Proposed

Litter bins should continue the black metallic motif. Gold detailing could be employed to add detail. They should be conveniently sized and located, for example within close proximity to benches, to encourage use. Litter bins should include the provision for collecting recyclable waste.

Existing Unacceptable Bins



Proposed Bins



8.4 Bollards

8.4.1 Existing Condition

Bollards were found to be high in quantity and have the greatest range of designs of all the items of street furniture, comprising a variety of styles, heights and material choices with no clear distinction as to why certain styles were used in certain locations.

Some bollards work well in their contexts, whereas other contribute to the cluttered feel of the conservation area with their materiality at odds with the traditional materials that would have been found in the town centre in the past. These include the stainless-steel bollards.

Whilst reflective strips have been employed successfully on some bollards, the use of brightly coloured reflective strips (such as the yellow and red varieties found on the stainless-steel bollards) is jarring. White reflective strip generally is more complementary.

8.4.2 Proposed

The placement and quantity of bollards needs to be carefully considered as their excessive use can be a contributor to the cluttered feel of space. Consequently, bollards should only be employed to demarcate important perimeters or boundaries, or to act as a design management tool for traffic.

Where required, unobtrusive black metallic structures with small ornate detailing would be appropriate.

Finally, the use of pop-up bollards is recommended at the southern end of the High Street. Their location is proposed within section 3 as part of the redesign of the High Street.

Existing Unacceptable Bollards



Proposed Bollards



Existing, Waterloo Court Lane S

8.5 Planters

8.5.1 Existing Condition

While the pockets of green infrastructure found in the conservation area are a welcome addition, the style of some planters sit in juxtaposition to the quality of the conservation area. Most awkwardly, the tall ivy climbing structures which sit incongruously to their setting.

In addition, the reduced quality and condition of the planters is beginning to become apparent, most obviously the timber planters.

8.5.2 Proposed

In keeping the black metal motif, simple clean-lined square planters are proposed. The range proposed comes in a variety of sizes and the edges are smoothed off for a public-realm appropriate finish.

Existing Unacceptable Planters



Proposed Planters

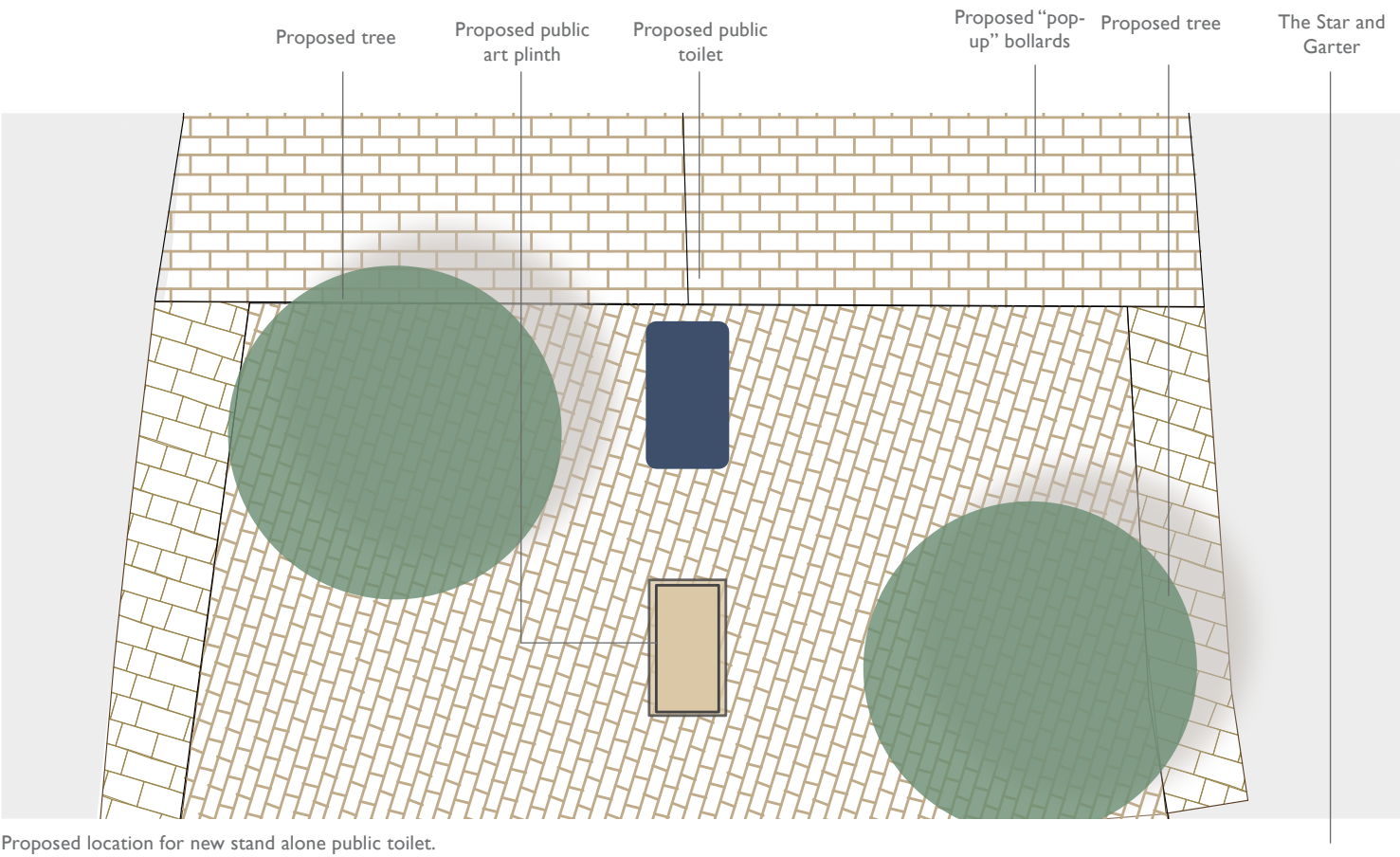


8.6 Public Toilets

The requirement for a new public toilet to the southern end of the High Street is to supplement the existing toilets found adjacent to The Chantry Centre car park and bus station. These would be in a similar style to the adjacent photograph. Because of its solid nature and tall stature, in addition to the key view towards The Guildhall which should not be blocked, its location has been carefully considered to minimise impact of the unit on the character of the High Street.

Consequently, a position for the unit is shown directly behind the newly proposed plinth (see public art section below for further details). The height and width of the plinth would act to screen the unit and furthermore, its location in the lower third of the High Street would mean that the upper two thirds would have uninterrupted views of The Guildhall and onwards to St Mary's Church. The estimate dimensions of the public toilet are approx.. 1.5m width x 3m depth x 2.5m height.

Proposed Toilet



Proposed location for new stand alone public toilet.

8.7 Lighting

8.7.1 Existing Condition

Andover employs a range of streetlight designs including single and double headed varieties in a range of finishes. While some examples fit harmoniously with Andover’s historic character, others - for example, the blue painted posts as in Waterloo Court Lane, detract from it.

8.7.2 Proposed

Victorian style lighting is adopted, to continue the style employed on the Victorian Jubilee fountain found at the heart of the conservation area. Further good examples exist, for example on Waterloo court when painted black.

Existing Unacceptable Lighting



Although the shape of the light is appropriate to the conservation area, the blue colour is not.

Proposed Lighting



Existing light found in Black Swan Yard Lane would be appropriate in all lanes and passageways.

Existing light found in Waterloo Court N in black is a style appropriate to the conservation area.

8.8 Bike Racks

Cycle racks should be located to encourage use, whilst not contributing to visual cluttering of the busier areas of the streetscape. While there is no Victorian bike stand as a precedent,, simple ‘Sheffield’ hoop stands can be made more harmonious by being black.

Existing Bike Racks



Proposed Bike Rack



8.9 Tree grilles and guards

As highlighted in chapter nine, many new street trees are proposed. Although in locations the density of underground services may render the planting of trees impossible unless they are within above ground planters, the trees strong preference is that trees are planted directly into the pavement. In these instances, the use of tree grilles and guards.

Good examples of tree guards already exist within the conservation area, for example within Waterloo Court Lane. Their black ‘railing’ design style is complementary to the other proposed street furniture elements and are a positive contribution to the traditional street scene.

Proposed Tree Guards and Grilles



8.11 Other

Other items such as BT feeder pillars and (typically now) underused telephone boxes continue to enhance the cluttered feel of the high street and detract from its character. As previously mentioned, a rationalisation exercise should consider the necessity of such items and where underused or their nature detracts from the character of the area, these should be removed.

Existing visually obtrusive furniture to be removed



8.10 Service Covers

Many service covers across the conservation area are not recessed, often straddle numerous material finishes and contrast against the surrounding materiality, which leads to a messy, careless feel.

Consequently, any new drainage and manhole covers should be recessed, to allow for hard surface material to be in-filled, to allow for a seamless finish. They should also ideally be situated wholly in one kind of material rather than straddling numerous, for a tidier finish. Furthermore, covers should be laid in the direction of, or 90 °to, the laying pattern. Random orientation should be avoided.

Existing service covers



Proposed service covers



Design Guidelines

- All benches to be made from black cast iron with ornate cast iron arms. The bench and back to be a high quality timber.
- All street furniture to be of the same style, black metal with only gold detail.
- All existing furniture that is not in keeping with the style proposed in this document should be removed.
- Street furniture needs to be rationalised and reduced so the streets are not overly cluttered with furniture.
-

8.12 Plaques

There is a broad spectrum of plaques within the town centre, ranging from surface mounted to wall mounted, metal and natural stone, painted or bare metal, In some cases, the plaques form part of a sequence. On no occasion has it been observed that they form a constraint, and all are to be retained.

Existing Plaques



8.13 Signage

8.13.1 Existing Condition

Street signage typically embraces a wide range of colours, materials and styles. Unlike various other elements of street furniture however, signage is designed to be seen. The detail design of signage is often led by regional or national legislation and is therefore beyond the scope of this report, other than to re-iterate that, over time, duplication of or retention of redundant signage can occur and therefore from both a functional and aesthetic perspective, less is more.

8.13.2 Proposed

Where signage can be controlled, when signaling local destinations within the town centre, the adoption of the traditional finger post in black painted metal should be adopted.

Existing Signage



Proposed Signage



8.14 Public Art and Sculpture

8.14.1 Existing Condition

Good quality public art was found across the conservation area, much of it reflecting the heritage of Andover, including mosaics and sculpture.

8.14.2 Proposed

Andover Town Council have expressed their desire for two new pieces of public art within the public realm.

The first piece, an ‘empty plinth’, would be built in a material sympathetic to the High Street such as limestone, would take the form of the Fourth Plinth in Trafalgar Square, which invites a rolling programme of temporary artwork. The proposed location of the plinth is proposed on page 33.

The second piece of public art, a Victorian style street clock, would act as an additional landmark along Bridge Street, at the intersection with the road to the Town Mills (see page 34 for proposed location). The simple columnar style in a black finish would be in keeping with the proposed street furniture.

In addition, the High Street would benefit from further public art, including as reported below, at the outer edges of the lanes and passageways. The art should also... help to create and reinforce Andover’s character, and could reflect its heritage, culture, and history. Be appropriately located and of a scale and mass that is sympathetic to its local context. Enhance the public realm and assist with legibility.

Furthermore, opportunities should be taken to engage local artists and groups with the design and creation of such pieces.

Existing Public Art



Proposed Public Art



8.16 Railings

8.16.1 Existing Condition

While the new modern railings alongside the riverside walk fit harmoniously within their context, there are examples such as the blue painted railings in the pocket park which jar against its natural setting.

8.16.2 Proposed

Black metallic railing should be adopted throughout the scheme, with pier caps matching the detailing found on the bollards to ensure consistency.

Existing railings around the perimeter of George Yard car park to be replaced by the black in keeping railings as proposed.

Any railings within the conservation area that are not black in colour and have a traditional look to them will need to be removed and replaced.

The black metal railings around the Andover Museum and Museum of the Iron Age should be replaced with suitable and in keeping railings. In the longstanding tradition of metalworking within Andover, the railings would ideally be sourced from a local supplier. Designs could then be mildly adapted to retain a sense of human individuality and craftsmanship.

Existing Railings



Proposed Railings



8.15 Bin Store Design

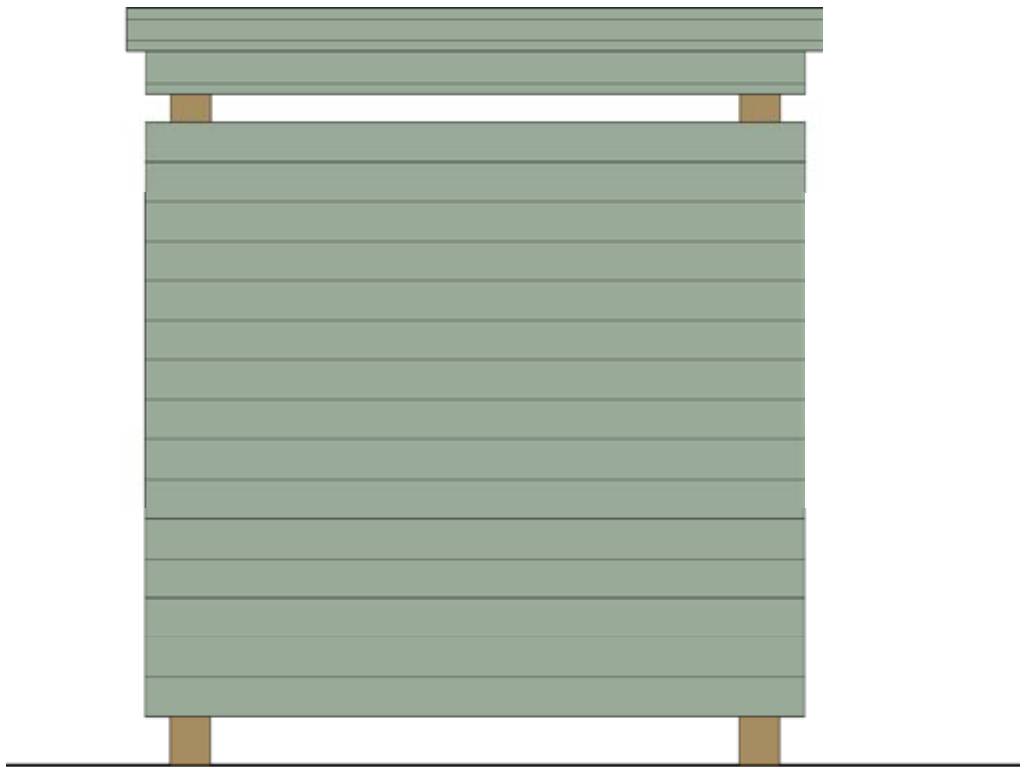
8.15.1 Existing siutation

It is a feature of Andover that a large majority of visitors arrive by car which results in the lanes and passageways becoming the primary entrances to the town centre. It is unfortunate that the point of entry is also the ‘back of house’ for many of the shops which sometimes results in full exposure of service yard and scattered commercial bins.

8.15.2 Proposed

Subject to vehicle access and sufficient provision for turning vehicles, the proposal is create a series of open- topped timber structures, that will screen the bins. These need to be designed allow an ease of use to encourage their adoption. To that end, the doors cold be generously wide and attached with self closure. They would need to be lockable for overnight security.

Existing Bin Storage



Design Guidelines

- All bins should be stored with an appropriately design bin store, to be in keeping with the historic setting.
- Bin stores to have bottle green coloured timber cladding.

9. Green Infrastructure

The value of street trees are manifold.They can:

- Screen less appealing views and frame and enhance beneficial ones
- Create habitats for wildlife and enhance bio-diversity
- Form green 'corridors' between existing green spaces
- Help carbon sequestration
- Provide shade in summer and slow wind vortices around buildings

While Andover already has green space close to the historic centre, it also has the capacity to incorporate to include street trees into the centre without compromising the important views.

The recommendation is that wherever possible, trees are introduced as planted directly into the pavement and this helps a more viable and sustainable tree to establish and grow. Conflicts with services can be minimized by using proprietary root barrier systems and establishing the safest locations with the use of cat scanners to locate cables.

Many robust and attractive varieties of trees are available for use in streets.These include Hornbeam, Lime, Birch, Maples, selected forms of Mountain Ash and Planes.The ultimate size in height and width are critical considerations in determining their locations.

Like any living thing, trees require management to thrive, particularly in the early years. However, as they mature, they Make a very positive contribution to the public realm and the benefit significantly outweighs the Initial investment.

Design Guidelines

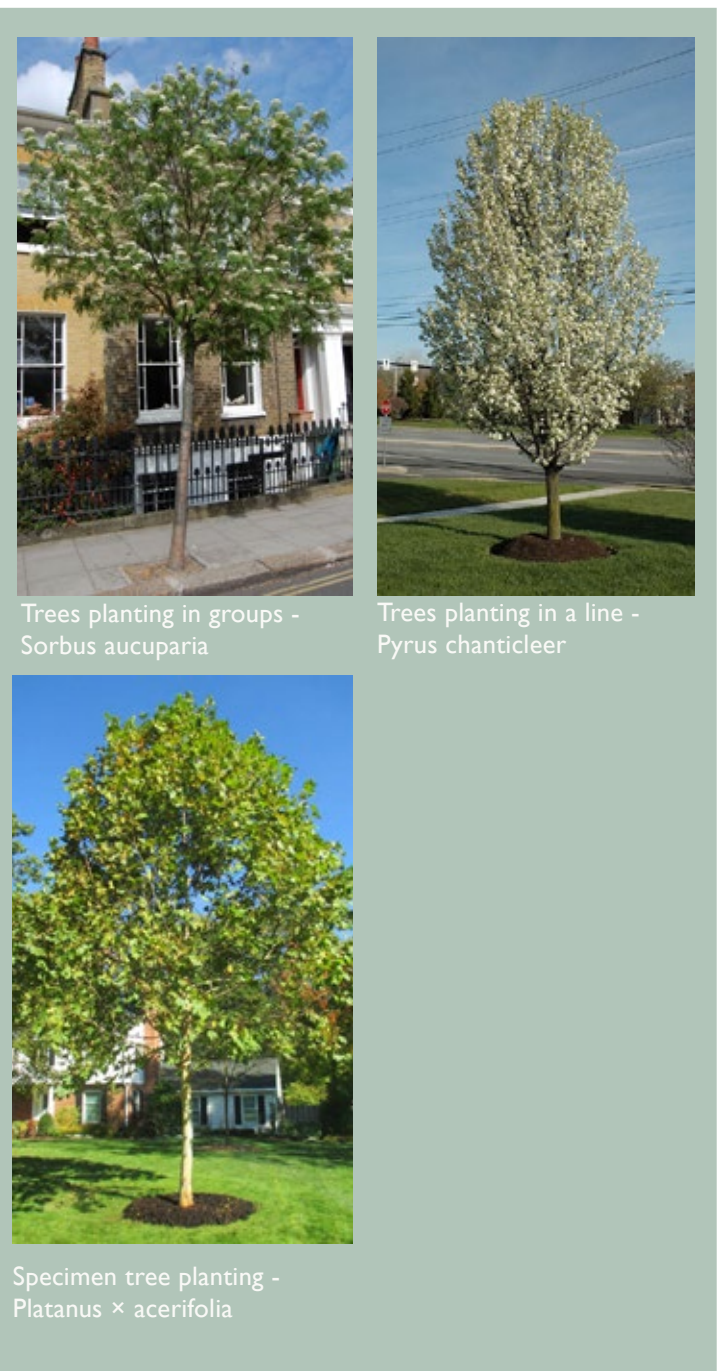
- The mature trees within the Conservation area must be retained.
- Any existing green space to be retained.
- Street trees along Bridge Street and High Street to be implemented as they will contribute important greening to the centre of Andover.



Important existing trees along Eastern Avenue



Example proposed tree planting



Section 3: Proposals

“

High quality surfacing for streets and pavements has a value far beyond the merely decorative. The careful distinctions of modular scale, material, colour and texture and the manner in which they are designed together can help link spaces, signal safe priority access for pedestrians or vehicles. By responding sensitively to the architecture and the spatial distribution of buildings, they play a significant role in establishing the Sense of Place

”

10.2 Street Type A - The High Street



Street Type A
Natural stone flag paving on footway, natural stone setts, natural stone block paving carriageway.
e.g. High Street

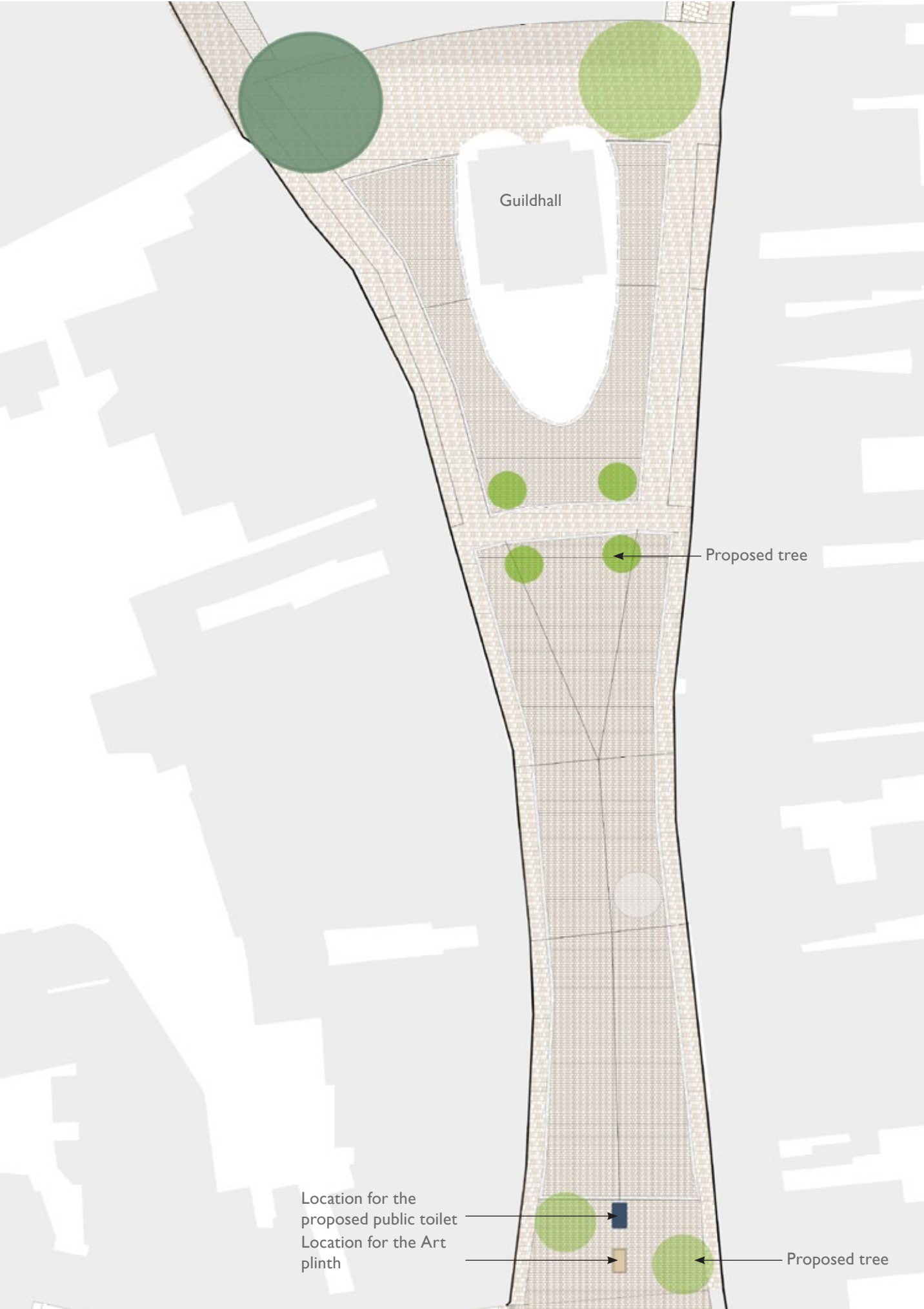


10.2.1 Surfacing

To help define the distinctive layout of the High Street, a clear distinction in materials has been made between it and the pedestrian lanes and passageways that provide access. The proposed materials follow traditional materials and bonding patterns. Because of its access status and use, the Upper High Street to Newbury Street, is treated as a pedestrian lane and is described separately below.

The arrangement of surfacing in flagstones, blocks and channels have a positive impact on the setting of buildings. Responding to the differing status of the buildings helps build a sense of place and the location plan shows one approach to deliver this. Rather than homogenise the paving into a single treatment, the layout responds to the spatial qualities within the High Street by creating a courtyard effect in front of the Star and Garter hotel and a framed setting for the Guildhall. In the middle section, a simpler layout is shown, with channels for drainage providing a traditional character.

Trees are located to frame views, soften the appearance of selected buildings, and provide dappled shade. Axial views of the guildhall are retained.



The High Street cont..

10.2.2 Built Form

The current relationship between the south facing facade of the Chantry Centre and the Guildhall is acknowledged to be a difficult juxtaposition of materials and architectural styles.

As part of the redevelopment proposals, if a more traditional facade treatment were to form the south facing elevations, this would evoke more closely the buildings and shop frontages that once existed here. As at the southern end, trees are proposed to increase the amenity for visitors as well as soften the new building elevations and thereby significantly improve the backdrop of the Guildhall by introducing a degree of visual separation. Metal bollards, tree grilles and bench seating all contribute to the language of the street furniture.

The proposed facades, while conceptual only do, nevertheless, show how deep colonnades, loosely modeled on those of traditional market towns, can help develop a comfortable microclimate for shoppers.

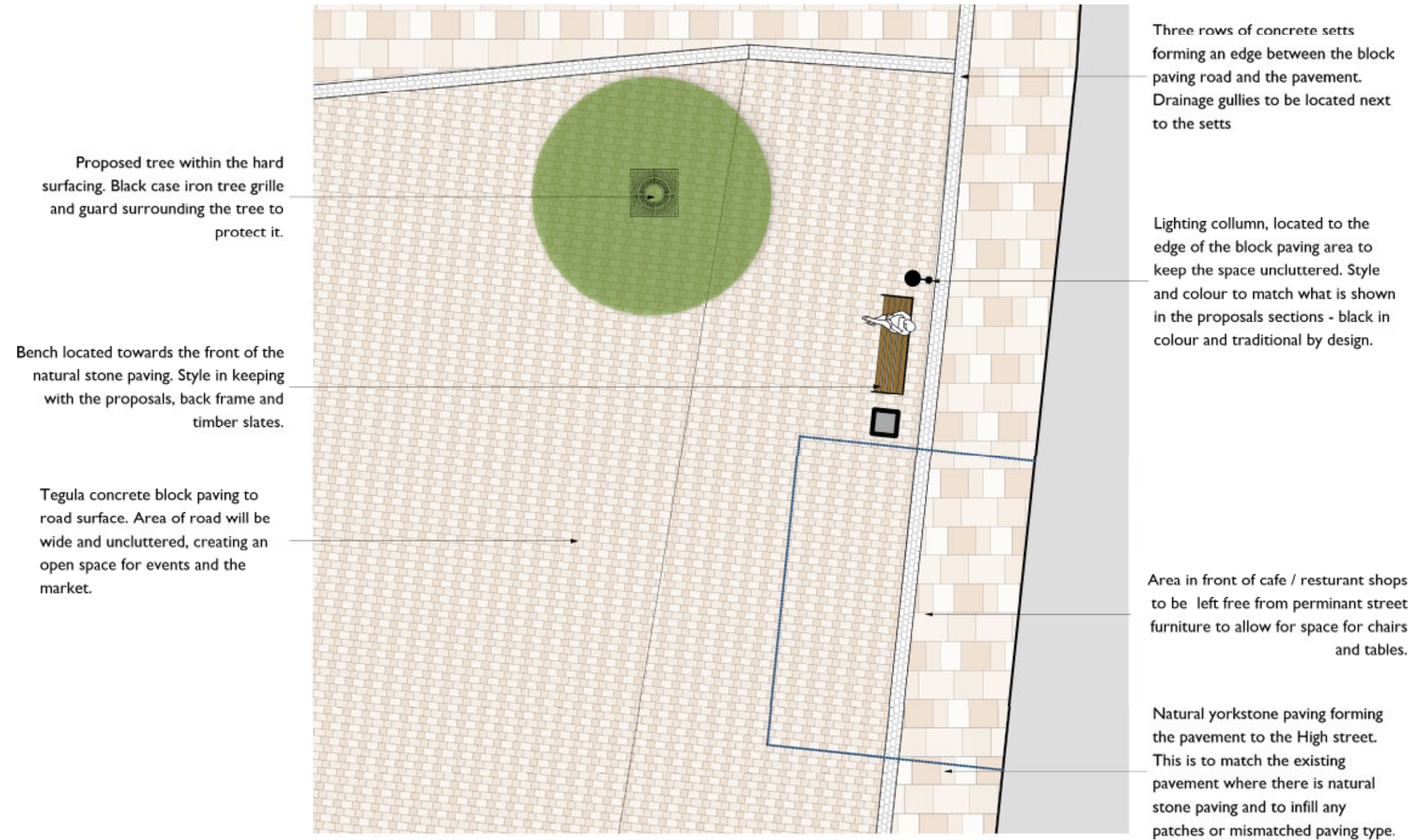
In addition, it is proposed that buildings on this elevation should be no greater than two and a half to three storeys and should be set back at least 16m behind the Guildhall., as shown in the section. This will allow the main elevation of the Guildhall, when viewed from the south, to be viewed against the sky. Setting the buildings back will also allow a more open sunnier space to develop as a more protected courtyard, one that will act as a natural spilling out space for users of the proposed relocated lights theatre.



Design Guidelines

- All surfacing to be high quality and in keeping with the existing high quality natural stone paving and setts.
- Proposed trees within the high street to enhance the greening within the street, but not distracting the views of the Guildhall.
- New buildings should be sympathetic to the scale of those surrounding them.
- New buildings must follow the historic building materials and design of the existing buildings within the Conservation Area.
- New buildings must be well built and attractively designed, in keeping with the historic setting.
- Building lines should enhance and respect the street scene and preserve it.

The High Street cont..



Design Guidelines

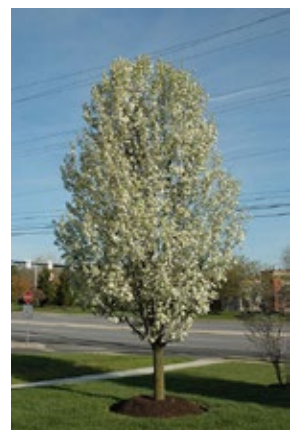
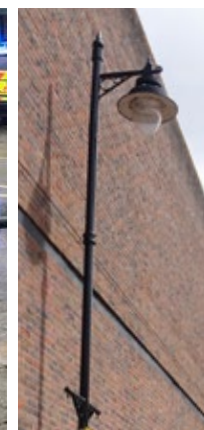
- Remove all unwanted clutter from the streets, including telephone boxes, and all furniture that is not inline with this report.
- Remove all ugly planters from the street.
- Remove all over sized lighting columns.
- Remove all concrete balls from the high street.
- All surfacing to be as shown in the report.
- All tree planting to be installed using a suitable crate system to to give the trees to best start in life.



This style of ornate bench to be located around the existing water fountain near the Guildhall only.



This style of bench to be located along the other parts of the high street.



Tree Planting
Pyrus chanticleer

10.2.3 The Andover Market

Andover benefits from regular markets, predominantly on a Thursday and a Saturday, with a programme of additional markets throughout the year, including a monthly Andover Artisan Market. Currently, stallholders are invited to bring their own gazebos, marquees, covers and tables, which can lead to a mismatched, messy feel.

However, in order to smarten the feel of the Andover Market, a single market stall covering should be used, as in Cirencester. Matching gazebos, marquees and covers helps to create a coherent and attractive market.

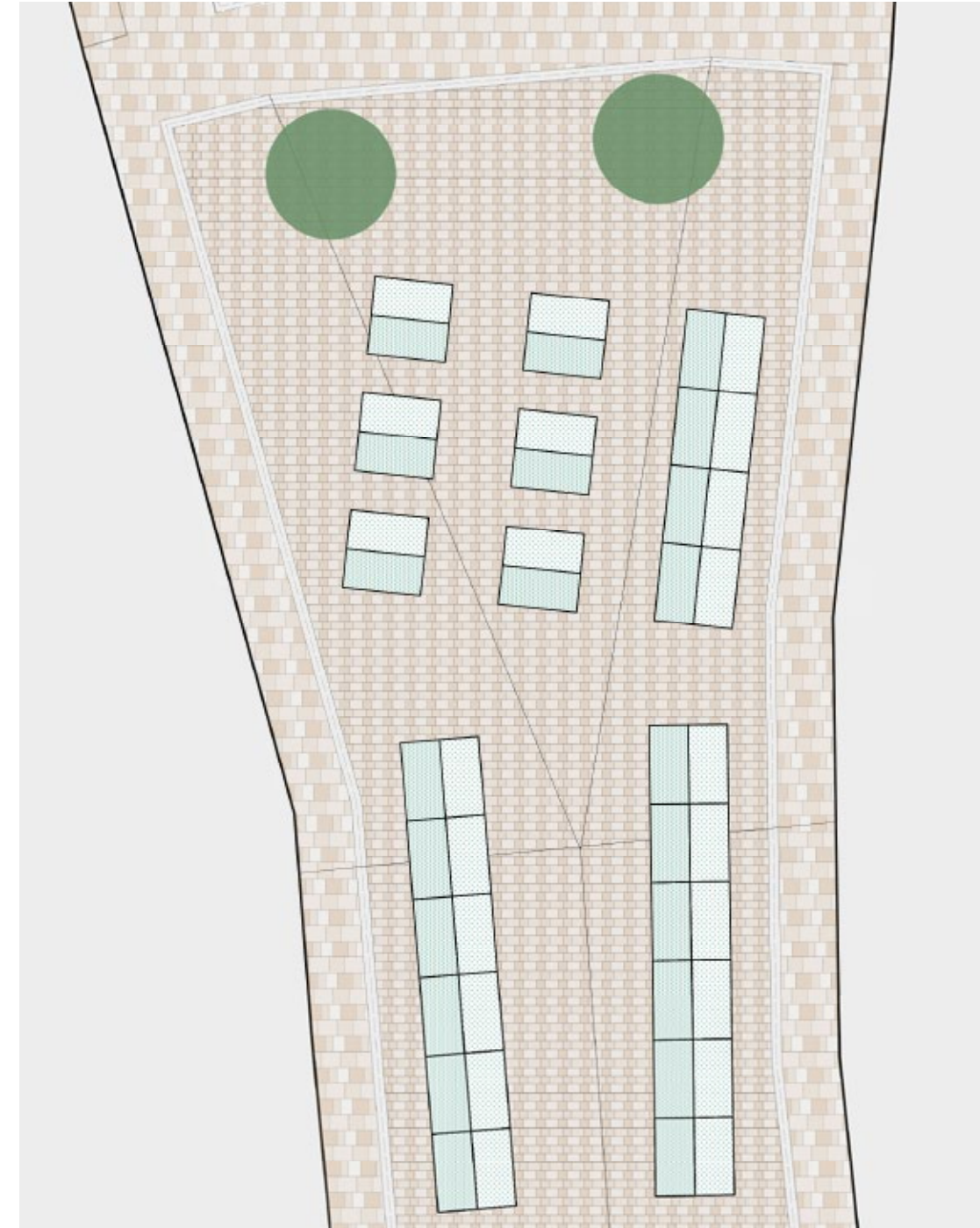
Existing images of Andover Markets



Proposed Market Stalls



Market Stall Layout



Design Guidelines

- The market stall should all match in their colour and stripe.
- The market stalls should all be laid out in appropriate straight lines and in a neat order.

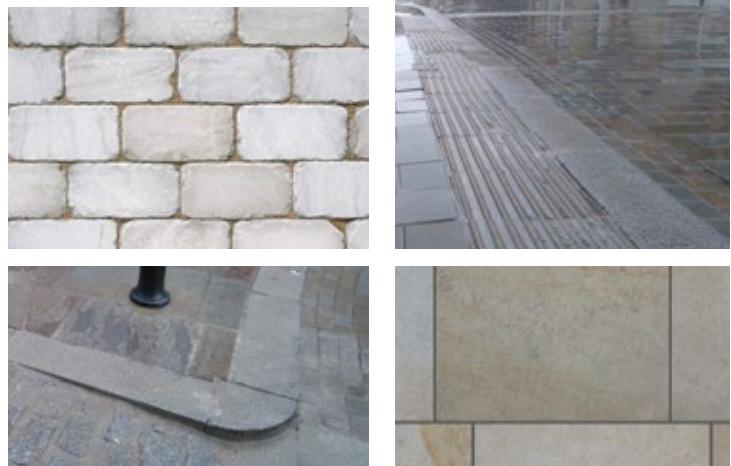
10.4 Street Type B - Bridge Street/High Street Intersection



10.4.1 Surfacing

In order to encourage the free movement of pedestrians and help connect the High Street to the shops, river, and parking south of Bridge Street, high quality sett paving is proposed. These indicate a reduced vehicle priority and, by increasing the ripple effect transmitted through to the driver, have a generally slowing effect. This can be further assisted by using raised tables, bollards, and tree planting to form traffic calming measures. The raised table effectively eliminates drop kerbs and in terms of signaling tis to guide dogs and visually impaired users, corduroy paving is proposed.

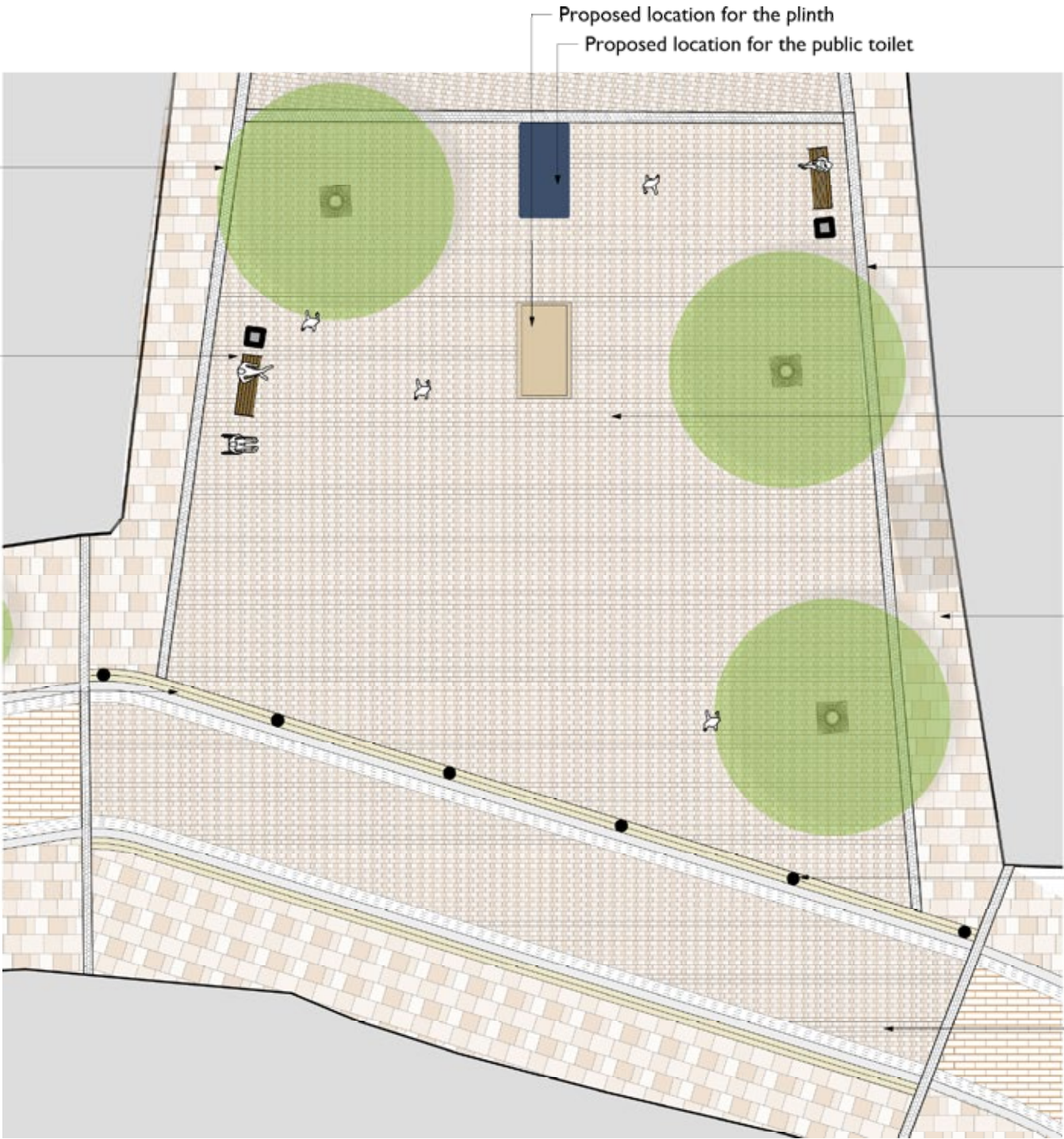
Street Type B
Natural stone flag paving on footway, corduroy paving, wide-top conservation kerb, natural stone sett paved carriageway & channel
e.g. Bottom of the High Street



Proposed tree within the hard surfacing. Black case iron tree grille and guard surrounding the tree to protect it.

Bench located towards the front of the natural stone paving. Style in keeping with the proposals, back frame and timber slates.

Corduroy paving creating a boundary to the road and the pedestrian open space



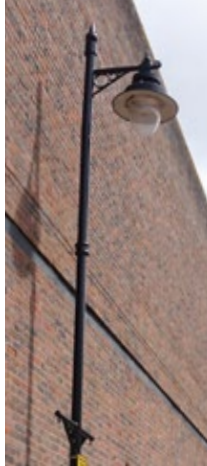
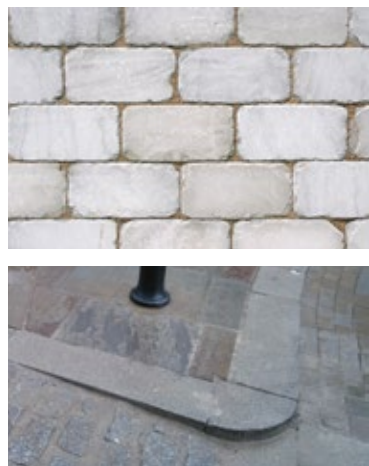
Three rows of natural stone setts forming an edge between the block paving road and the pavement. Drainage gullies to be located next to the setts

Natural stone block paving. Laid in stretcher course across the width of the road.

Natural yorkstone paving forming the pavement to the High street. This is to match the existing pavement where there is natural stone paving and to infill any patches or mismatched paving type.

Cast iron bollards, some to be removals for emergency vehicals, deliveries and market traders

Natural stone block paving. Laid in stretcher course across the width of the road.



10.3 Street Type C

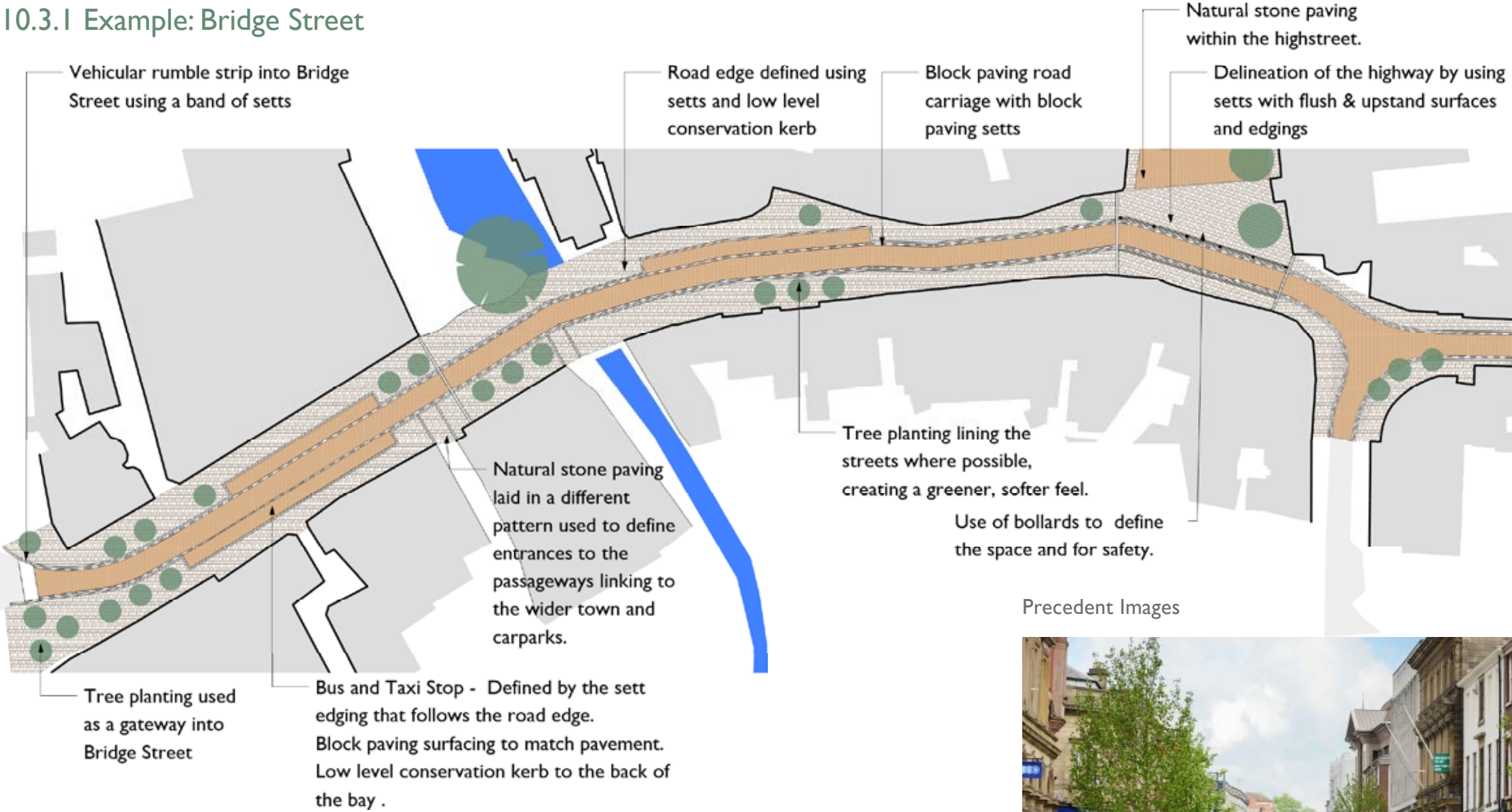


Street Type C
Natural stone flag paving on footway, wide-top conservation kerb, block channel & carriageway
e.g. Bridge Street



10.3.1.1 Surfacing
In order to provide a visual cue of pedestrian priority along streets that are used by both pedestrians and vehicles, the vehicular carriageway has been standardised to 4.5 m, which has the added advantage of widening the appearance of the pavements. This provides a strategic opportunity to “green” the space and introduce trees into the landscape. The presence of trees in such a space can also act as a traffic calming measure.

10.3.1 Example: Bridge Street



Precedent Images



Street Type C - Bridge Street

Proposed tree within the hard surfacing. Black case iron tree grille and guard surrounding the tree to protect it.

Taxi / Loading bay in concrete block paving, with a conservation kerb dividing the parking from the pavement

Concrete block paving to road with three rows of concrete setts.

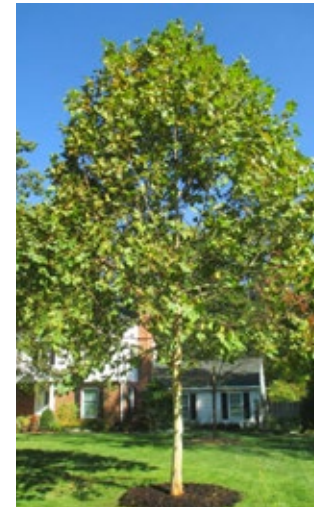
Natural yorkstone paving forming the pavement to the High street. This is to match the existing pavement where there is natural stone paving and to infill any patches or mismatched paving type.

Bench located within the pavement with enough space for circulation in front and behind. Style in keeping with the proposals, back frame and timber slates.

The lighting columns on bridge street are to remain as existing where they are black and traditional in style.



Bridge Street section, looking eastwards towards The Star and Garter



Design Guidelines

- Remove all unwanted clutter from the streets, including telephone boxes, and all furniture that is not inline with this report.
- Kerbs to have a maximum upstand of 75mm.
- all surfacing to be as shown in the report.
- Decreet lighting to be included on the building elevations or retain appropriate lighting columns.
- All tree planting to be installed using a suitable crate system to give the trees to best start in life.

10.5 Street Type D - The Upper High Street



Street Type D
Natural stone flag paving on footway, natural stone sett paved carriageway & channel
e.g. Upper High Street



10.5.1 Surfacing

This street is characterized by the relatively close proximity of the building facades, lending it a more intimate character than the more expansive High Street to the south. It has also developed a busier atmosphere as the footfall of the units generates more pedestrian activity immediately outside the shops.

The redevelopment of the Chantry Centre will have the potential to change the character of the street and the use of high quality natural stone flags and setts for the central section will continue at a smaller scale the quality and character of the High Street. In most respects, the surfacing will reflect the traditional layout of pavements on either side of a central single carriageway. However, it will differ in that the it is not anticipated to have vehicular traffic other than emergency or service vehicles and therefore there will not be kerbs. In this respect, it evokes at a larger scale the character of the pedestrian lanes and passageways.

10.5.2 Built Form

It would be entirely appropriate and in keeping with other historic market towns to employ a narrow portico frontage, an approach proposed on the Upper High Street side of the building at the corner of the Lower High Street and Upper High Street.

Precedent Images



Section, corner of the High Street and Upper High Street, looking northwards

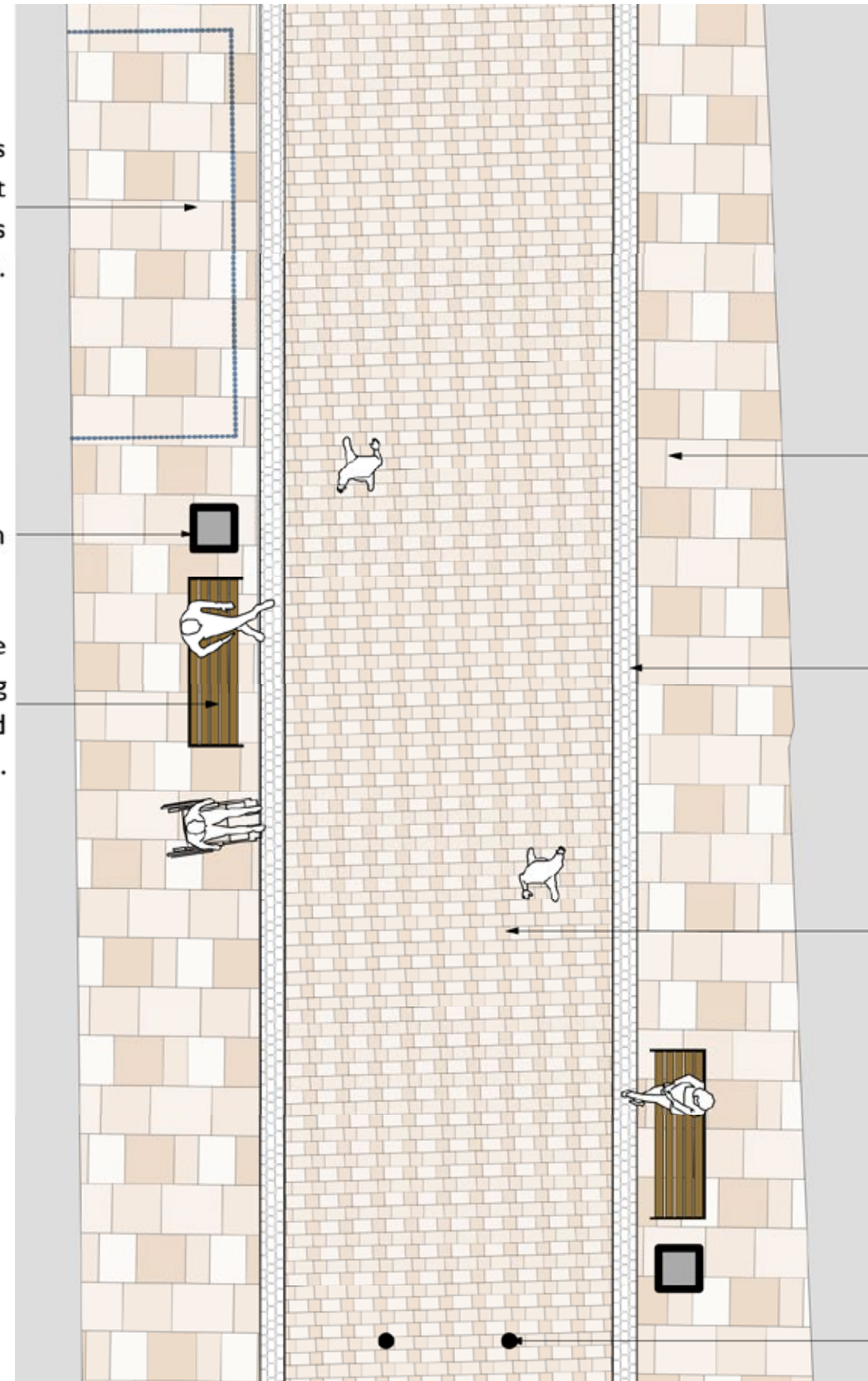
- ### Design Guidelines
- Remove all unwanted clutter from the streets, including telephone boxes, and all furniture that is not inline with this report.
 - No kerbs with the Upper High Street.
 - all surfacing to be natural stone paving as shown.
 - Descreet lighting to be included on the building elevations or retain appropriate lightingn collumns.

The Upper High Street cont,.

Area in front of cafe / restaurant shops to be left free from permanent street furniture to allow for space for chairs and tables.

Proposed bin

Bench located towards the front of the natural stone paving. Style in keeping with the proposals, back frame and timber slates.



Natural Yorkstone paving forming the pavements to the upper high street

Three rows of natural stone sets forming an edge between the block paving road and the pavement. Drainage gullies to be located next to the sets

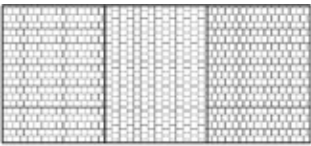
Natural stone block paving, 4m wide. Laid in stretcher course across the width of the road.

Black cast iron removable bollards, located at the beginning and the end of the upper high street. Bollard style to be in keeping with the style proposed. Removable for access to emergency vehicles.

10.6 Street Type E - Lanes and Passageways

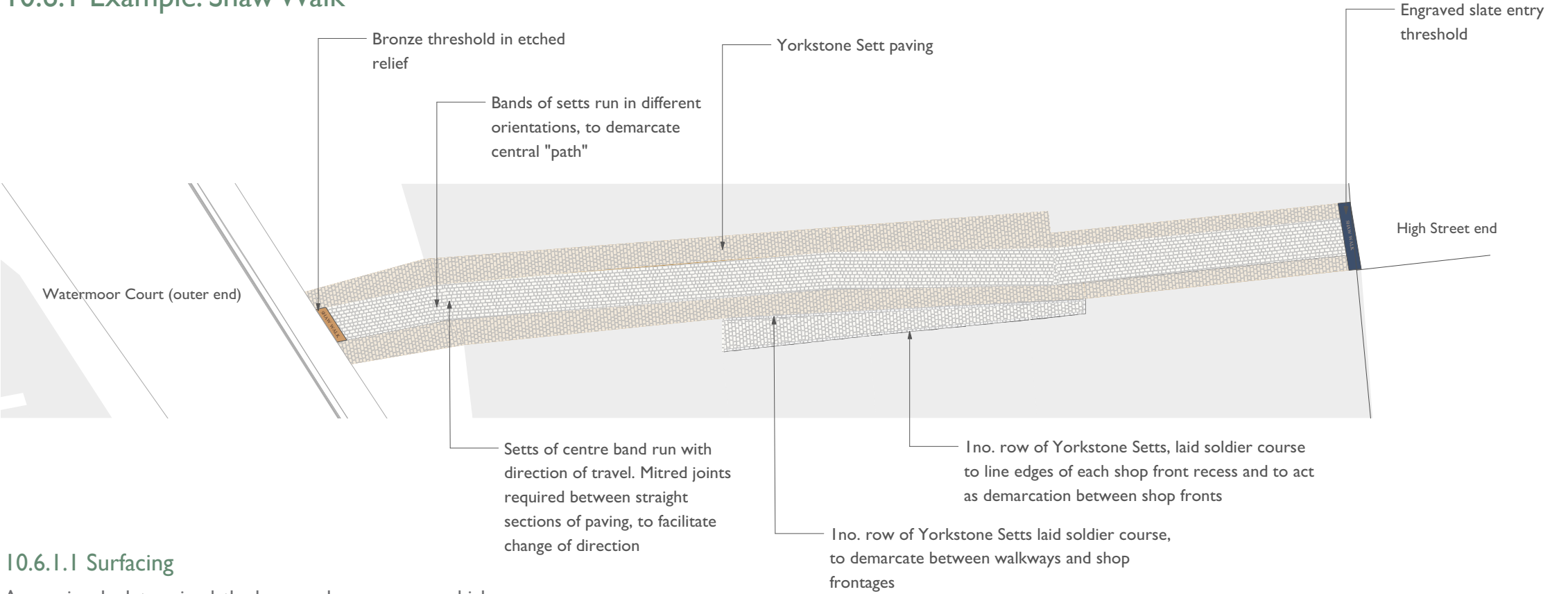


Street Type E - Lanes
e.g. Union Street and upper high street
Natural yorkstone setts



Bands of Natural Sett Paving in different orientations

10.6.1 Example: Shaw Walk



10.6.1.1 Surfacing

As previously determined, the lanes and passageways which form access routes from the periphery of the conservation area into the High Street act as a threshold into the historic core. Although the lanes are demarcated by arched signways at their High Street ends, no formal demarcation occurs on the peripheral thresholds. When combined with the “back of house” feel of these outer spaces, the lanes and passageways are often lost within the mass of buildings.

Consequently, the demarcation of the lanes on their outer end is significantly important too. To improve the legibility of these spaces, bronze etched thresholds are proposed to mark the entryway, highlighting the name of the lane, in addition to new arched signage. To reflect this, etched slate threshold plaques are proposed at the High Street end.

Furthermore, to formalise the lanes into destinations rather than just thoroughfares, new rectangular tumbled sett paving is proposed in a colour such grey in keeping with lanes of other historic market towns. Each lane should follow a similar pattern, separated into three bands with the outer two bands running at an orientation of 90° to the central band. This is to demarcate a central path.



Outer end : Broze threshold with the name of the lane in relief over an etched darker background.



High Street end : A slate slab, engraved with the name of the lane.



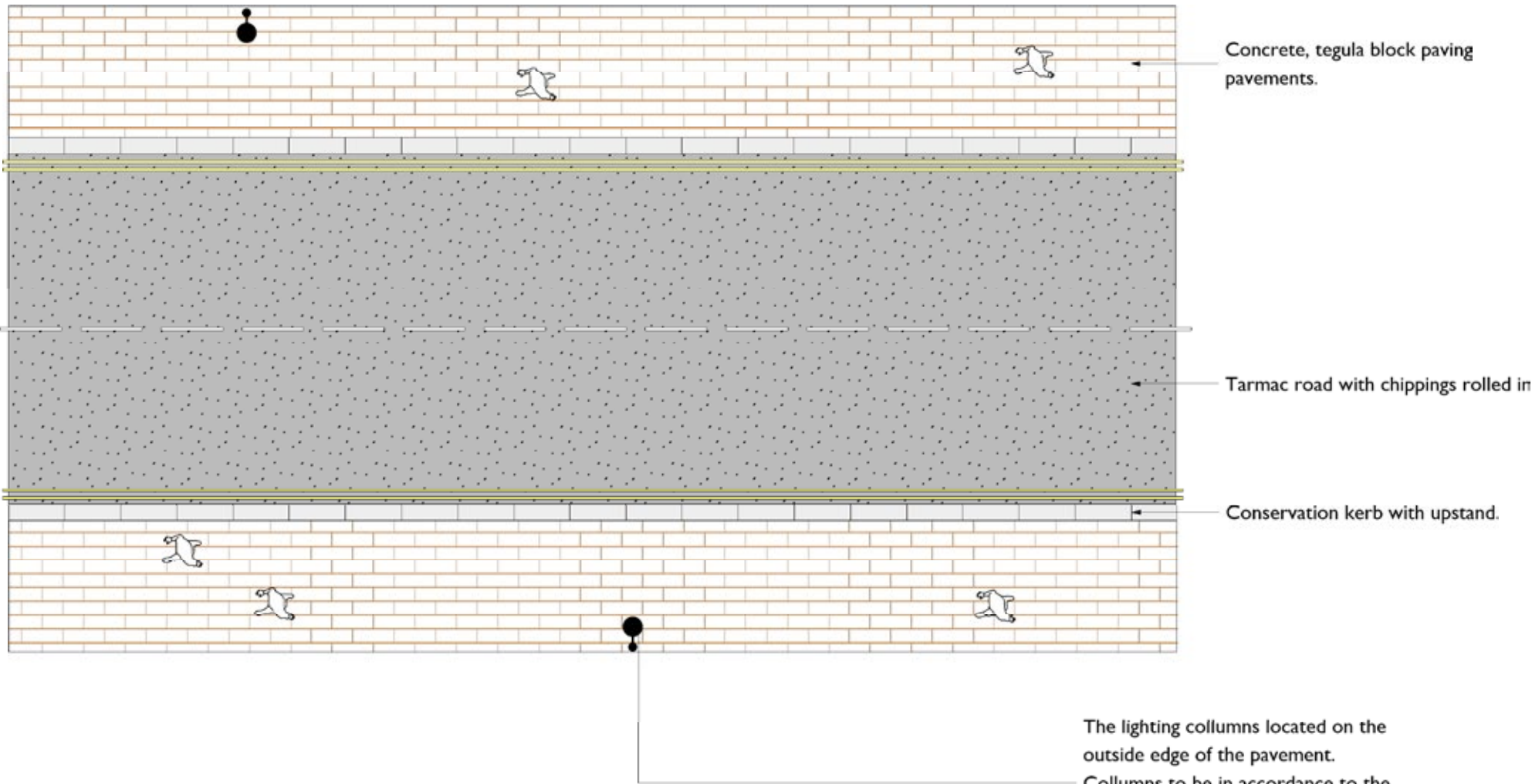
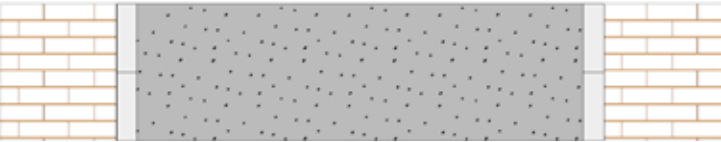
Design Guidelines

- Remove all unwanted clutter from the streets, and all furniture that is not inline with this report.
- all surfacing to be natural stone sett paving as shown.
- Descreet lighting to be included on the building elevations or use appropriate lighting collumns as shown.

10.7 Street Type F



Street Type F
Block paving on footway, wide-top conservation kerb, tarmac & rolled chippings finish to carriageway
e.g. Eastern Avenue



The lighting columns located on the outside edge of the pavement.
Columns to be in accordance to the highways specification, but in the same style as the report specifies.

10.7.1.1 Surfacing
As a result of the greater frequency of traffic, the generally higher vehicle speeds and fewer of shops and therefore pedestrian footfall, Eastern Avenue is treated in a more utilitarian manner.
The pavements are shown as reconstituted blocks with conservation grade kerbs but the carriageway surface is proposed as bound tarmac with a rolled in chipping finish.

- Design Guidelines
- All tarmac roads to have chippings rolled in.
 - All kerbs to be conservation kerb or to the same standard.
 - All pavements to be a concrete block paving.
 - All furniture to be inline with the report.

11. Conclusion

Like many other market towns, Andover is experiencing a fundamental shift in the way that people purchase consumer goods and services. The trend towards on-line shopping, which was already making significant in-roads into the viability of the traditional High Street, was accelerated in 2020 by Covid-19. This in turn heightened the need for traditional retail centres to evolve even more rapidly to assess their future role: which of the retail outlets are likely to remain viable, will the growth in leisure destinations prove to be sustainable and to what extent will the considerable volume of town centre buildings be able to accommodate future residential demand? Another outcome from the pandemic is the value of accessible public open space and associated green infrastructure.

Every indication is that creating town centres as enjoyable and attractive destinations and making them a good place to live will create the most viable, sustainable future. In this regard, Andover already has many qualities to develop and this report strongly advocates that enhancing the public realm by bringing a consistently high quality to the streetscape which will, in addition, enhance the built environment and improve accessibility. Where development takes place within the Conservation Area, protecting the character of the historic centre by designing buildings that are sympathetic to their context and both desirable and appropriate for future residential use is in the best interests for the future of Andover.